

Bella Vista at the National

PRELIMINARY DEVELOPMENT PLAN

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HIGHWAY 45

Design Notes:

Water Quality Area - Design Intent: water quality area to be naturalistic with wetland trees, visible from entry drive and golf course

Landscape Design Intent:
 A. fill slopes below buildings - introduce tree massings to mitigate building height from drive approach, low shrub under-story, (grow-low sumac), pine straw mulch.
 B. light wells at building entry - upright deciduous (columnar aspen), and evergreen trees.
 C. lawn areas - irrigated lawn focused along entry drive and flat areas around buildings where slopes allow for proper maintenance.
 D. parking lot tree requirements - 1 tree per 10 spaces has been provided in all parking areas. Where there are not enough parking islands to accommodate required trees, trees are placed in adjacent landscape areas.

Pool Amenity Area - Design Intent: infinity edge pool and spa features to take advantage of dramatic topography, ADA access to pool, area to include fire feature(s), grill stations, cabana, television, variety of furniture types, access to adjacent interior parking and clubhouse amenity area above, Re: Arch.

SITE LEGEND

- A. PROJECT ENTRY
- B. PROJECT IDENTIFICATION SIGN
- C. COMMERCIAL PAD SITE
- D. PARKING
- E. ENTRY DRIVE
- F. WATER QUALITY AREA
- G. ADJACENT GOLF COURSE
- H. RESIDENTIAL STRUCTURE, RE: ARCH
- I. LIGHT WELL, TYP., RE: ILLUSTRATIVE VIEW
- J. PEDESTRIAN ACCESS BRIDGE, TYP.
- K. COVERED PARKING
- L. RETAINING WALL, RE: CIVIL
- M. CAR CARE CENTER
- N. WALKING TRAIL (wood mulch surface - woodland character)
- O. DOG PARK
- P. FIRE ACCESS DRIVE, RE: CIVIL
- Q. POOL AMENITY AREA, RE: ILLUSTRATIVE VIEW
- R. TRASH COMPACTOR



PRELIMINARY DEVELOPMENT PLAN

A NEW MULTI-FAMILY DEVELOPMENT FOR:
Bella Vista at the National

MO Highway No. 45 & Lake Crest Lane
 Parkville, Missouri
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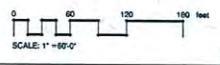


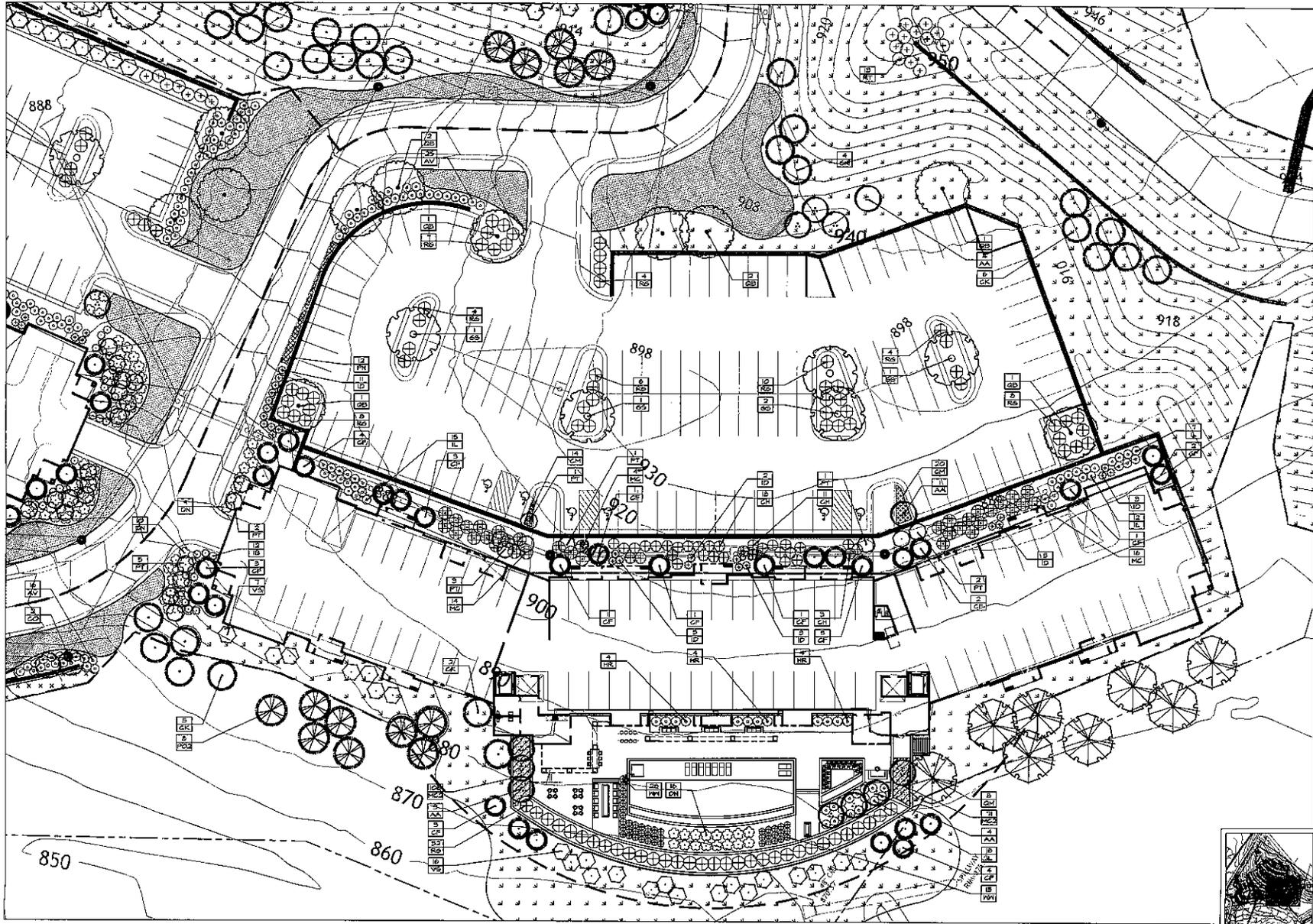
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 (816) 732-2541

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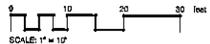
ILLUSTRATIVE PLAN
 PROJECT: DATE: JUNE 16, 2015
 SHEET NUMBER:

L000





A LANDSCAPE PLAN
SCALE: 1"=10'-0"
PLAN



PRELIMINARY DEVELOPMENT PLAN

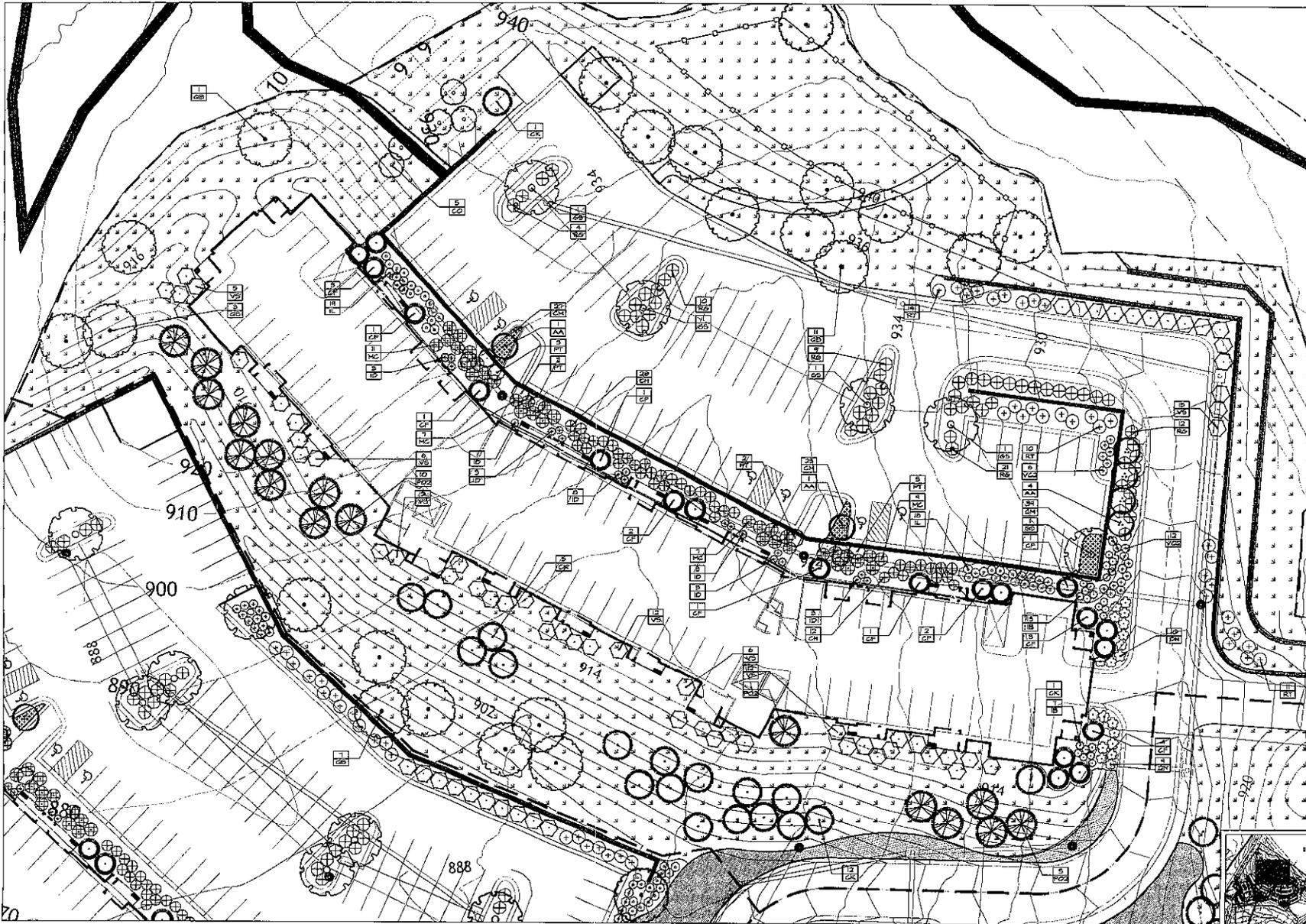
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Parsippany, NJ 07054
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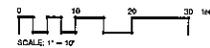
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REVISIONS:

LANDSCAPE PLAN	DATE:	
PROJECT:	DATE:	
DRAWN BY:	DATE:	
CHECKED BY:	DATE:	
L.101		



A LANDSCAPE PLAN
SCALE 1"=10'-0" PLAN



PRELIMINARY DEVELOPMENT PLAN

A NEW MULTI-FAMILY DEVELOPMENT FOR:
Bella Vista at the National

400 Highway No. 48 & Lake Crest Lane
Parkville, Missouri

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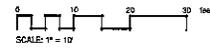
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(816) 792-5691

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LANDSCAPE PLAN	DATE: 11-11-15	L102
PROJECT: BELLA VISTA AT THE NATIONAL	SCALE: 1"=10'-0"	
ENTER NUMBER:		



A LANDSCAPE PLAN
SCALE: 1"=10'-0"
PLAN



PRELIMINARY DEVELOPMENT PLAN

A NEW MULTIFAMILY DEVELOPMENT FOR:
Bella Vista at the National

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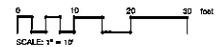
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(816) 732-6951
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REVISIONS:

LANDSCAPE PLAN	DATE:	PROJECT NUMBER:
	08-27-15	L104



A LANDSCAPE PLAN
SCALE: 1"=10'-0"
PLAN



PRELIMINARY DEVELOPMENT PLAN

A NEW MULTIFAMILY DEVELOPMENT FOR:
Bella Vista at the National

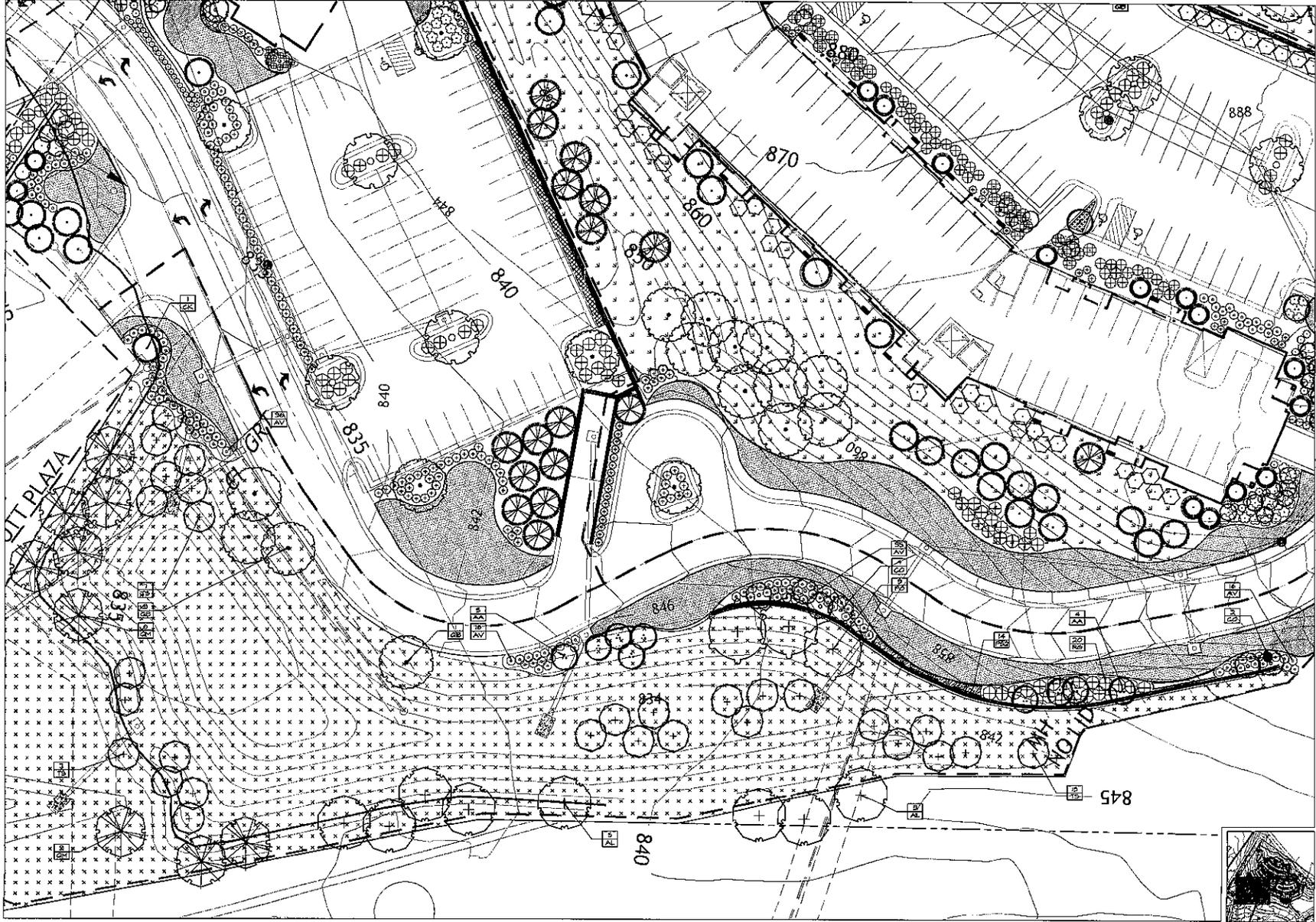
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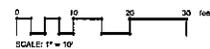
115 W. Genoa
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REVISIONS:

LANDSCAPE PLAN	DATE:	
PROJECT:	BY:	
REVIEW NUMBER:	DATE:	
		L103



A LANDSCAPE PLAN
SCALE: 1"=10'-0" PLAN



PRELIMINARY DEVELOPMENT PLAN

A NEW MULTI-FAMILY DEVELOPMENT FOR:
Bella Vista at the National

MO Highway No. 45 & Lake Crest Lane
Parkville, Missouri

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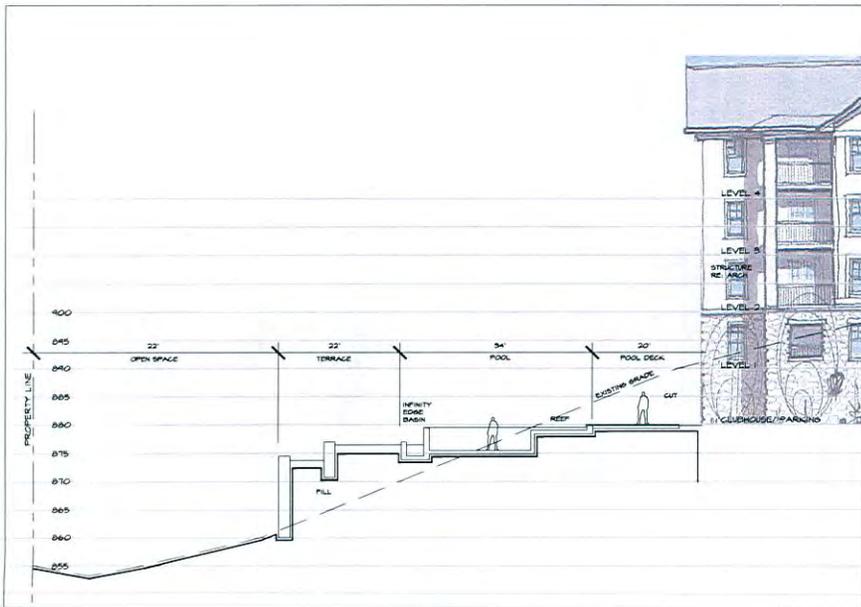
LANDSCAPE PLAN	L 105
PROJECT:	
DATE:	
SHEET NUMBER:	



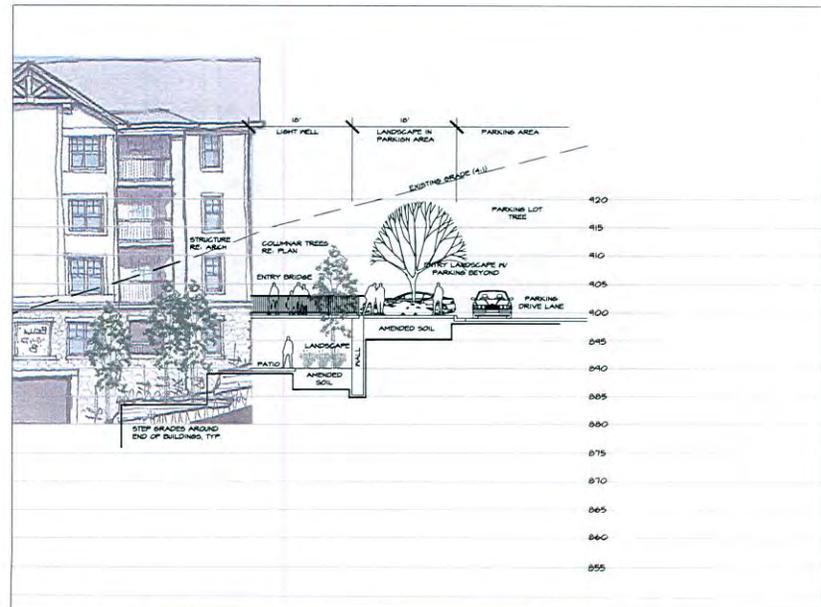
D ILLUSTRATIVE POOL VIEW 1
SCALE: NTS



C ILLUSTRATIVE POOL VIEW 2
SCALE: NTS



B SECTION - CONCEPTUAL POOL CENTERLINE
SCALE: 1"=10'-0"
PLAN



A SECTION - CONCEPTUAL LIGHT WELL/ BUILDING ENTRY
SCALE: 1"=10'-0"
PLAN

PRELIMINARY DEVELOPMENT PLAN

A NEW MULTIFAMILY DEVELOPMENT FOR:
Bella Vista at the National

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Parkville, Missouri

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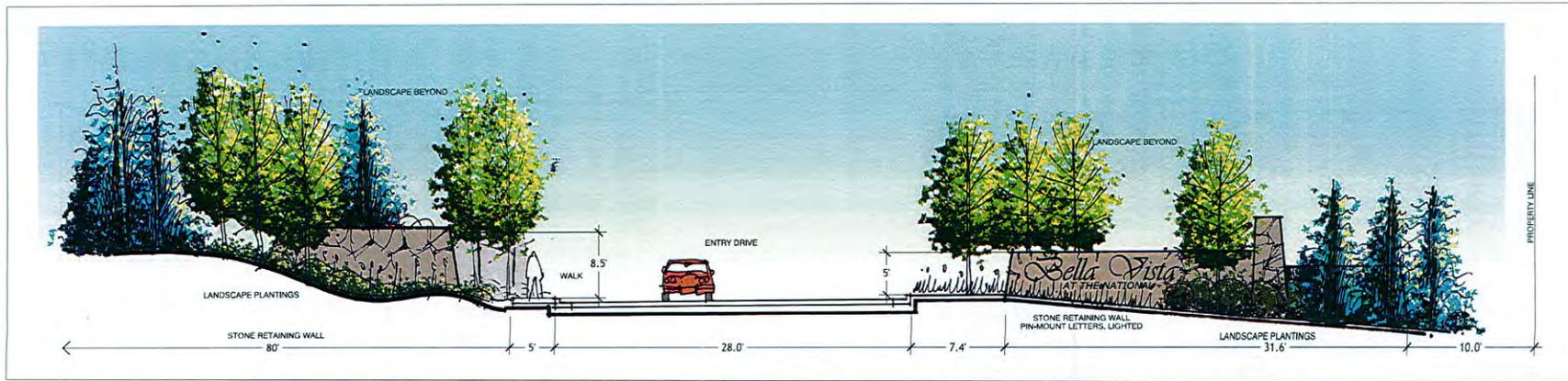
105 W. Kansas
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REVISIONS:

ILLUSTRATIVE VIEWS	
PROJECT DATE	
SHEET NUMBER	L202



B ILLUSTRATIVE VIEW - PROJECT IDENTIFICATION ENTRY MONUMENT
SCALE: NTS



A ILLUSTRATIVE ELEVATION - PROJECT IDENTIFICATION ENTRY MONUMENT
SCALE: NTS

PRELIMINARY DEVELOPMENT PLAN

A NEW MULTI-FAMILY DEVELOPMENT FOR:
Bella Vista at the National

MO Highway No. 45 & Lake Crest Lane
Pawcatuck, Missouri

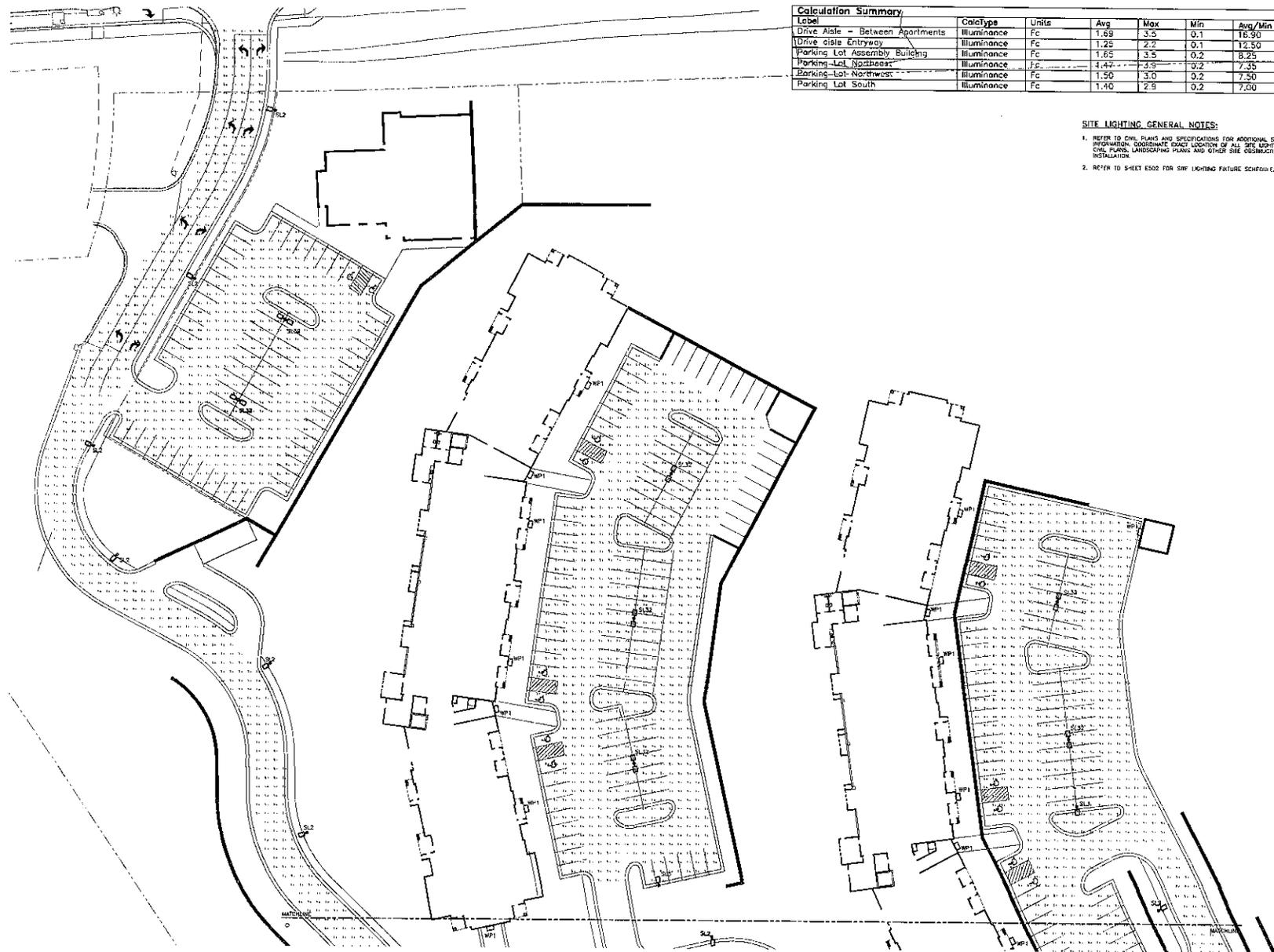
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REVISIONS:

ILLUSTRATIVE VIEWS
IDENTIFICATION
PROJECT NUMBER:
DATE:
SHEET NUMBER:
L203



Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
Drive Aisle - Between Apartments	Illuminance	Fc	1.69	3.5	0.1	16.90	35.00
Drive aisle Entryway	Illuminance	Fc	1.25	2.2	0.1	12.50	22.00
Parking Lot Assembly Building	Illuminance	Fc	1.65	3.5	0.2	8.25	17.50
Parking Lot Northeast	Illuminance	Fc	4.44	3.9	0.2	7.35	19.50
Parking Lot Northwest	Illuminance	Fc	1.50	3.0	0.2	7.50	15.00
Parking Lot South	Illuminance	Fc	1.40	2.9	0.2	7.00	14.00

SITE LIGHTING GENERAL NOTES:

- REFER TO SITE PLANS AND SPECIFICATIONS FOR ADDITIONAL SITE LIGHTING INFORMATION. COORDINATE EXACT LOCATION OF ALL SITE LIGHTING FIXES WITH CIVIL, PLUMBING, LANDSCAPING PLANS AND OTHER SITE OBSTRUCTIONS PRIOR TO INSTALLATION.
- REFER TO SHEET E502 FOR SITE LIGHTING FIXTURE SCHEDULE.

1 SITE LIGHTING AND PHOTOMETRIC PLAN
SCALE: 1"=30'-0"

DAVID D. HANKE

DAVID D. HANKE
LICENSE # E-155140822

HENDERSON ENGINEERS
AN ILLINOIS LIMITED LIABILITY PARTNERSHIP
100 W. KILBURN
LIBERTY, MISSOURI 64068
TEL: 816.783.5200
WWW.HENDERSONENGINEERS.COM

NOTE:
ALL SUBCONTRACTORS SHALL BE RESPONSIBLE FOR REVIEWING ALL DRAWINGS IN THE SET PRIOR TO PLACING BID.

A NEW APARTMENT UNIT PLAN FOR:
Bella Vista at the National

Lakewood Lane
Parkville, MO 64152
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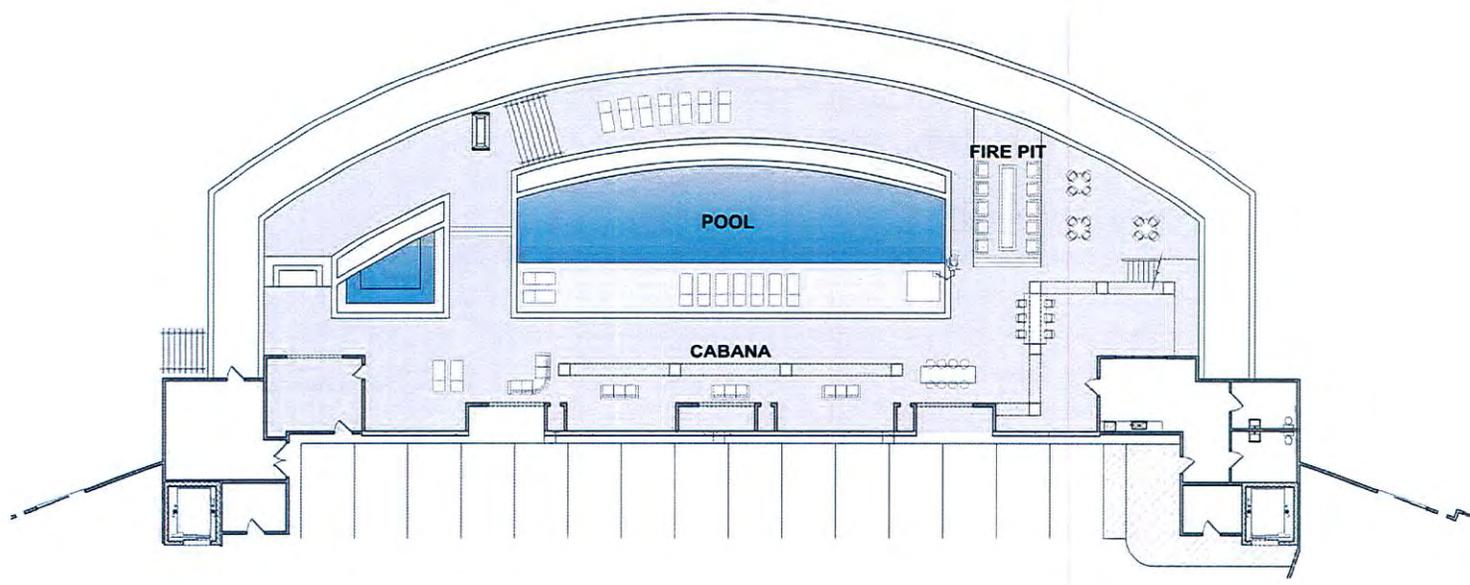
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REVISIONS:

CIVIL SITE LIGHTING PLAN AND PHOTOMETRICS

PROJECT: DATE: 09.18.2019

SHEET NUMBER: **E500**



A1.0

CONSTRUCTION DOCUMENTS

A NEW APARTMENT UNIT IN AN OLD
 Bella Vista at the National

Lakewood, MD 21112
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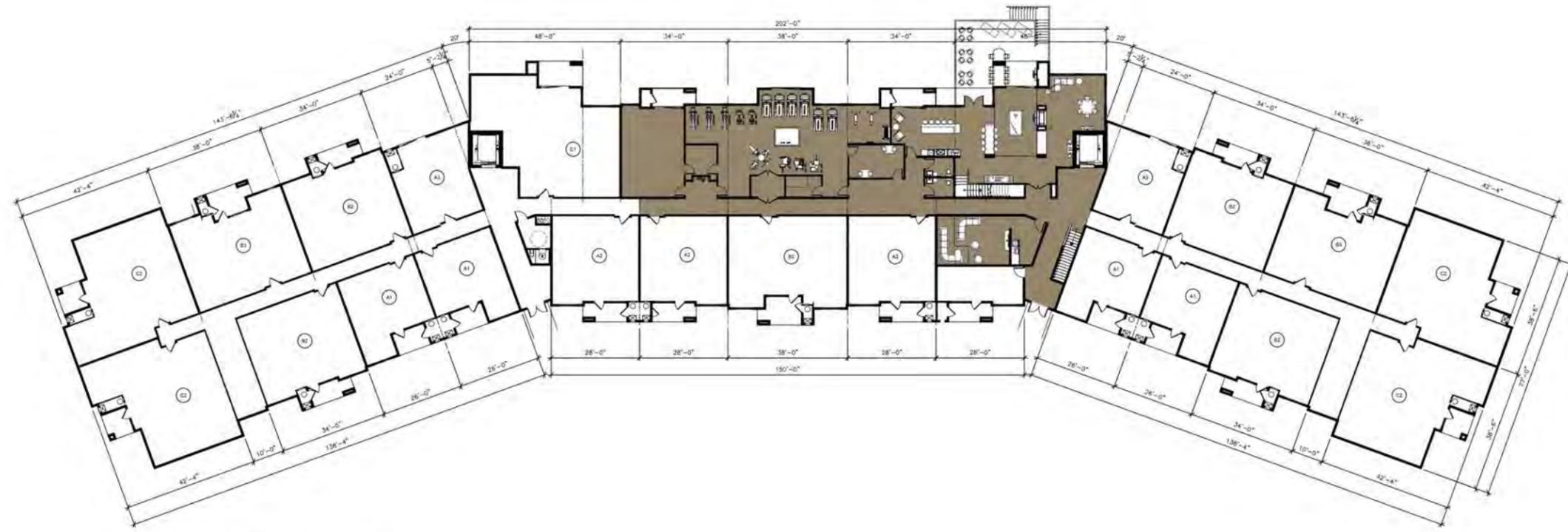


J. PRICE ARCHITECTURE, INC.
 220 W. Kappa
 Suite C
 Chevy Chase, MD 20815
 301-953-7000

REVISIONS

PROJECT	DATE	BY
APARTMENT UNIT	10/15/15	J.P.
SHEET NUMBER		

NOTE:
 ALL SUBCONTRACTORS SHALL BE
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 PLACING.



1 Typical Building First Floor Plan
SCALE: 1/16" = 1'-0"



NOTE:
ALL SUBCONTRACTORS SHALL BE
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CONSTRUCTION DOCUMENTS

A NEW APARTMENT UNIT PLAN FOR:
Bella Vista at the National

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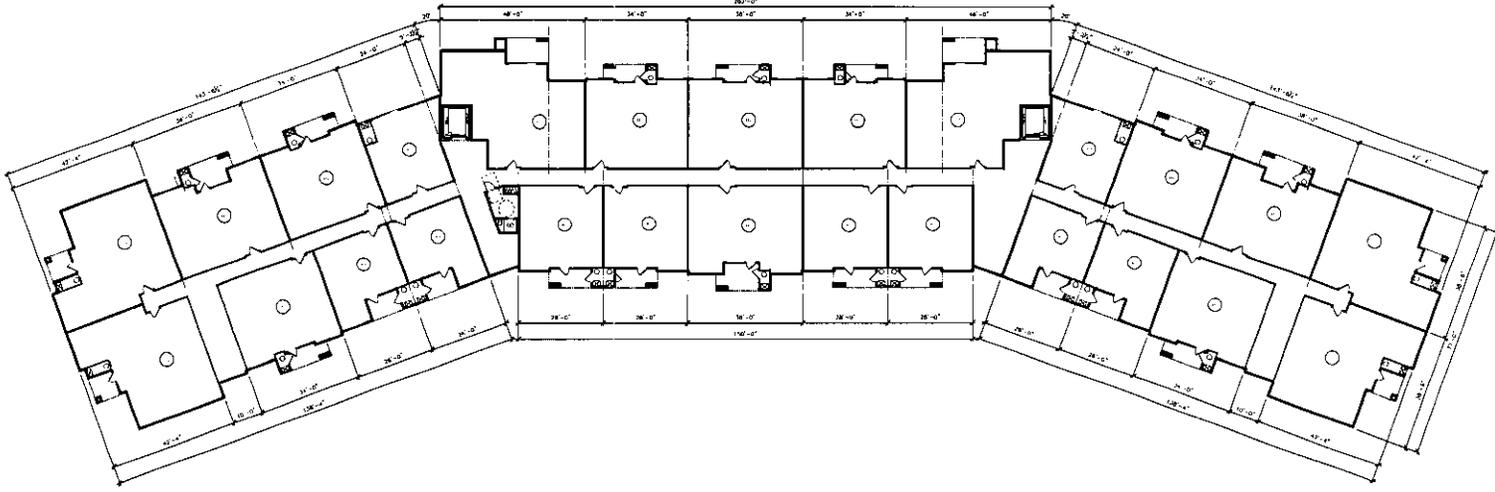
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REVISIONS

PROJECT:	DATE:
SHEET NUMBER:	NO. 1 OF 1

A1.1

A1.3



1 Typical Building Third Floor Plan
SCALE: 1/8"=1'-0"

CONSTRUCTION DOCUMENTS

A NEW APARTMENT UNIT PLAN FOR
Bella Vista at the National

LANSING LANE
PAPAYAKI, MD 24152

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J. J. Jones
ARCHITECTURE, INC.
1000 N. WASHINGTON BLVD.
SUIT 100
LANSING, MD 24152
301.281.1111

NO.	DATE	BY
1	08/14/2019	JOHN J. JONES

NOT TO SCALE. THIS PLAN IS FOR INFORMATION ONLY. THE ARCHITECT ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF THE INFORMATION PROVIDED HEREIN.



1 Building Parking Elevation
SCALE: NTS



2 Building Garage Elevation
SCALE: NTS

PRELIMINARY DEVELOPMENT PLAN

A NEW MULTI-FAMILY DEVELOPMENT FOR:
Bella Vista at the National

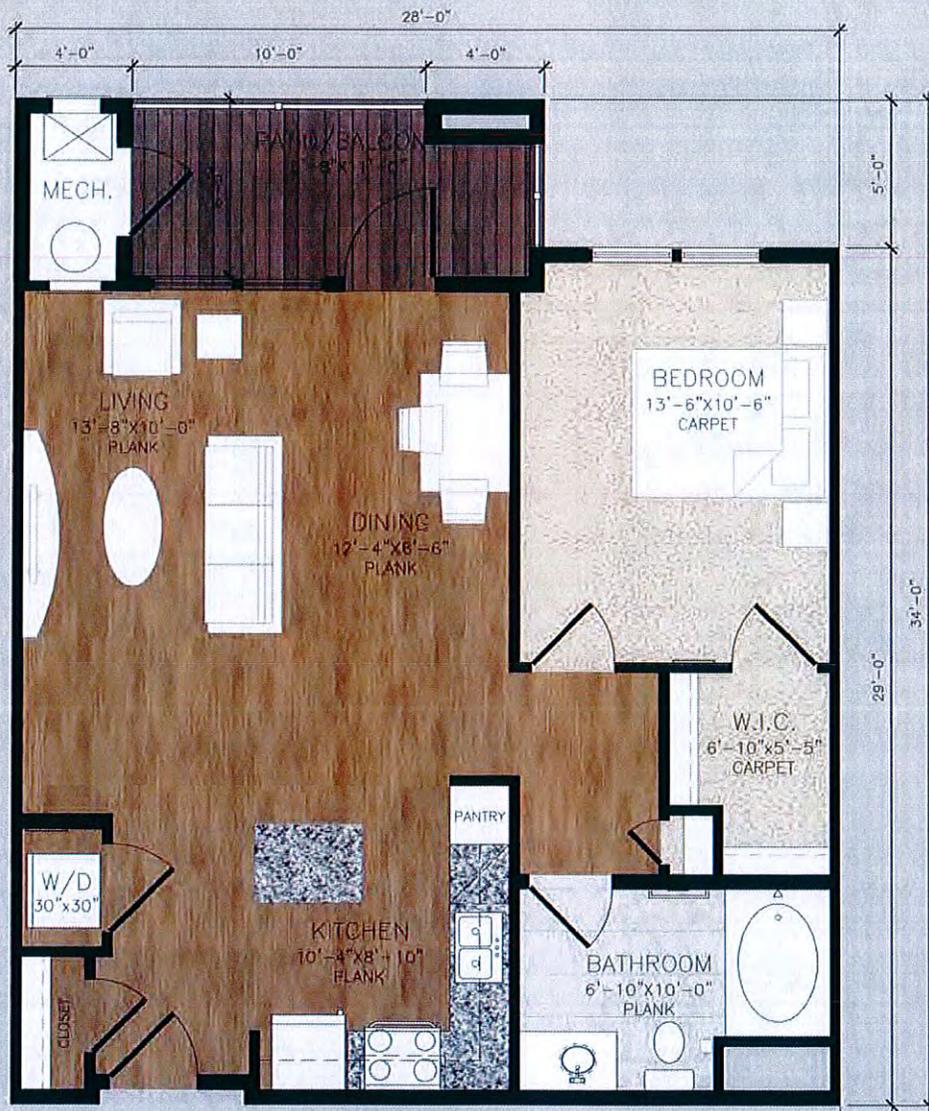
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REVISIONS:

PROJECT	DATE	BY
SHEET NUMBER	A4.0	



A2 (22)
 MARKET NET - 816 SF
 MARKET GROSS - 900 SF

2 Unit "A2" Layout
 SCALE: 1/4" = 1'-0"

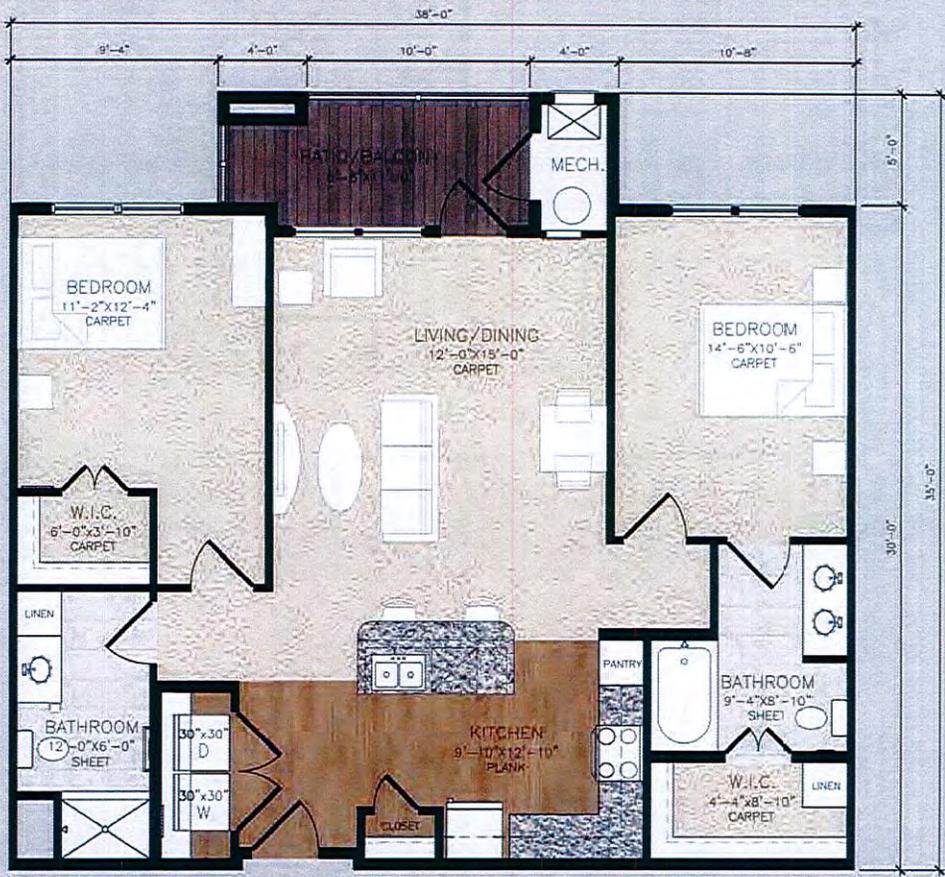
PRELIMINARY DESIGN DOCUMENTS

UNIT A2 FLOOR PLAN	
PROJECT	DATE
A2.02	April 20, 2013
SHEET NUMBER:	
A2.02	



A NEW APARTMENT UNIT PLAN FOR:
Bella Vista at the National

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Unit "B2" Layout
SCALE: 3/16" = 1'-0"

B2 (48)
MARKET NET - 1146 SF
MARKET GROSS - 1228 SF

PRELIMINARY DESIGN DOCUMENTS

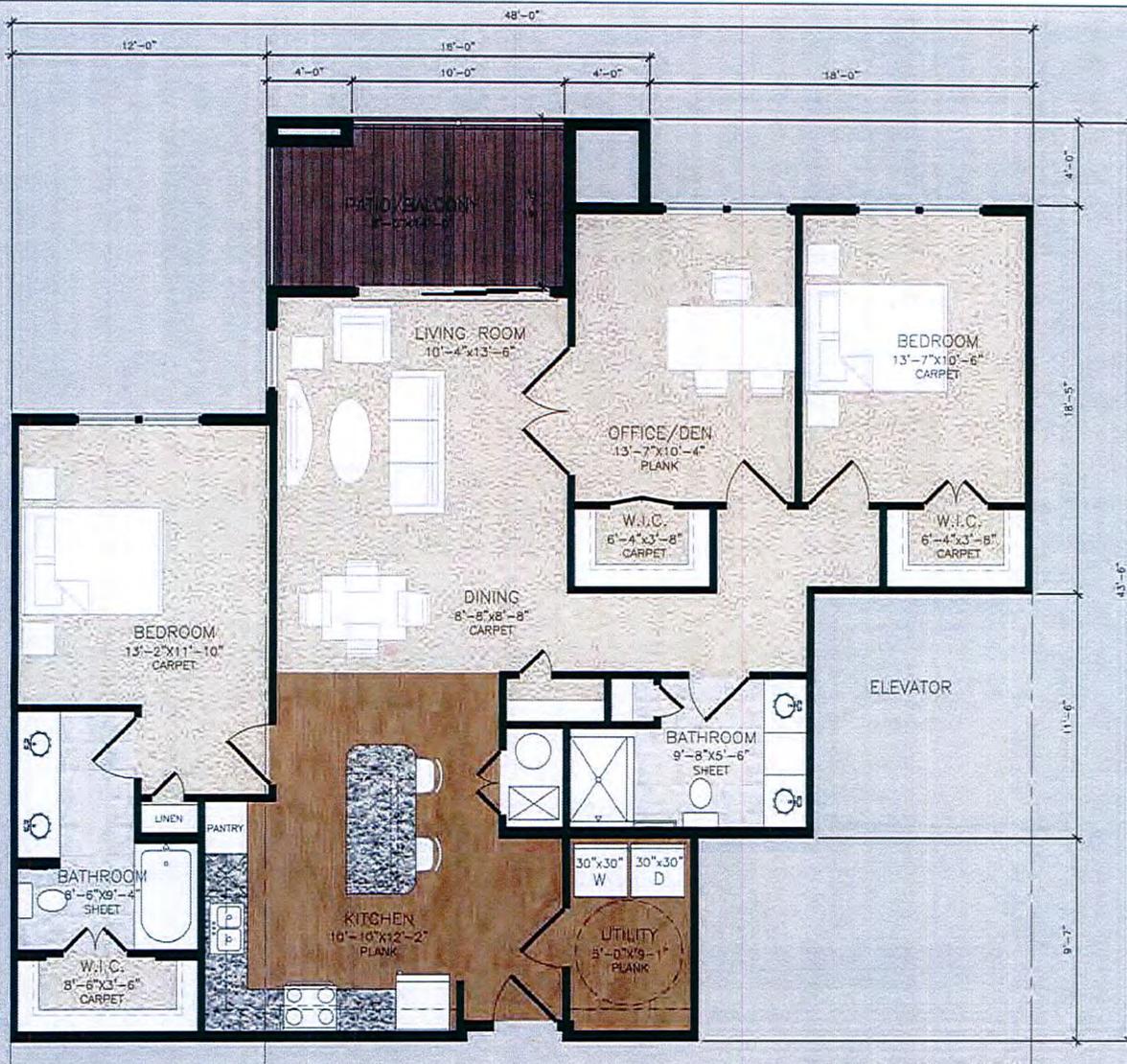
A NEW APARTMENT UNIT PLAN FOR:
Bella Vista at the National



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UNIT B2 FLOOR PLAN	DATE: _____
PROJECT: _____	SCALE: _____
SHEET NUMBER	A2.10



8 Unit "C1" Layout
 SCALE: 3/16" = 1'-0"

C1 (22)
 MARKET NET - 1450 SF
 MARKET GROSS - 1564 SF

PRELIMINARY DESIGN DOCUMENTS

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UNIT C1 FLOOR PLAN	DATE	DATE
PROJECT		
SHEET NUMBER		

A2.17



9 Unit "C2" Layout
 SCALE: 3/16" = 1'-0"

C2 (48)
 MARKET NET - 1338 SF
 MARKET GROSS - 1407 SF

PRELIMINARY DESIGN DOCUMENTS

A NEW APARTMENT UNIT PLAN FOR:
Bella Vista at the National



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UNIT C2 FLOOR PLAN	DATE	SCALE
PROJECT	DATE	SCALE
SHEET NUMBER		
A2.18		

Unit Design	Unit Type	MKT Net	MKT Gross	Building 1											
				First			Second			Third			Fourth		
				Qty.	Net	Gross									
A1	1BR/1BA BALCONY	678	752	4	2,712	3,008	4	2,712	3,008	4	2,712	3,008	4	2,712	3,008
A2	1BR/1BA BALCONY	816	900	3	2,448	2,700	1	816	900	2	1,632	1,800	2	1,632	1,800
A2S	1BR/1BA SUNROOM	900	900	-	-	-	1	900	900	2	1,800	1,800	2	1,800	1,800
A3	1BR/1BA SUNROOM	718	718	2	1,436	1,436	2	1,436	1,436	2	1,436	1,436	2	1,436	1,436
B1	2BR/2BA BALCONY	1,028	1,108	4	4,112	4,432	4	4,112	4,432	6	6,168	6,648	6	6,168	6,648
B1S	2BR/2BA SUNROOM	1,108	1,108	-	-	-	-	-	-	-	-	-	-	-	-
B2	2BR/2BA BALCONY	1,146	1,228	3	3,438	3,684	4	4,584	4,912	4	4,584	4,912	4	4,584	4,912
C1	3BR/2BA BALCONY	1,450	1,564	1	1,450	1,564	1	1,450	1,564	2	2,900	3,128	2	2,900	3,128
C2	3BR/2BA BALCONY	1,338	1,407	4	5,352	5,628	4	5,352	5,628	4	5,352	5,628	4	5,352	5,628
Total:				21	20,948	22,452	21	21,362	22,780	26	26,584	28,360	26	26,584	28,360

Building 1	Total Units:	94	(+1 MODEL)
Building 1	Total MKT Net:	95,478	
Building 1	Total MKT Gross:	101,952	

Unit Design	Unit Type	MKT Net	MKT Gross	Building 2											
				First			Second			Third			Fourth		
				Qty.	Net	Gross									
A1	1BR/1BA BALCONY	678	752	4	2,712	3,008	4	2,712	3,008	4	2,712	3,008	4	2,712	3,008
A2	1BR/1BA BALCONY	816	900	4	3,264	3,600	2	1,632	1,800	2	1,632	1,800	2	1,632	1,800
A2S	1BR/1BA SUNROOM	900	900	-	-	-	2	1,800	1,800	2	1,800	1,800	2	1,800	1,800
A3	1BR/1BA SUNROOM	718	718	2	1,436	1,436	2	1,436	1,436	2	1,436	1,436	2	1,436	1,436
B1	2BR/2BA BALCONY	1,028	1,108	6	6,168	6,648	6	6,168	6,648	6	6,168	6,648	6	6,168	6,648
B1S	2BR/2BA SUNROOM	1,108	1,108	-	-	-	-	-	-	-	-	-	-	-	-
B2	2BR/2BA BALCONY	1,146	1,228	4	4,584	4,912	4	4,584	4,912	4	4,584	4,912	4	4,584	4,912
C1	3BR/2BA BALCONY	1,450	1,564	2	2,900	3,128	2	2,900	3,128	2	2,900	3,128	2	2,900	3,128
C2	3BR/2BA BALCONY	1,338	1,407	4	5,352	5,628	4	5,352	5,628	4	5,352	5,628	4	5,352	5,628
Total:				26	26,416	28,360	26	26,584	28,360	26	26,584	28,360	26	26,584	28,360

Building 2	Total Units:	104
Building 2	Total MKT Net:	106,168
Building 2	Total MKT Gross:	113,440

Unit Design	Unit Type	MKT Net	MKT Gross	Building 3											
				First			Second			Third			Fourth		
				Qty.	Net	Gross									
A1	1BR/1BA BALCONY	678	752	4	2,712	3,008	4	2,712	3,008	4	2,712	3,008	4	2,712	3,008
A2	1BR/1BA BALCONY	816	900	4	3,264	3,600	2	1,632	1,800	2	1,632	1,800	2	1,632	1,800
A2S	1BR/1BA SUNROOM	900	900	-	-	-	2	1,800	1,800	2	1,800	1,800	2	1,800	1,800
A3	1BR/1BA SUNROOM	718	718	2	1,436	1,436	2	1,436	1,436	2	1,436	1,436	2	1,436	1,436
B1	2BR/2BA BALCONY	1,028	1,108	6	6,168	6,648	6	6,168	6,648	6	6,168	6,648	6	6,168	6,648
B1S	2BR/2BA SUNROOM	1,108	1,108	-	-	-	-	-	-	-	-	-	-	-	-
B2	2BR/2BA BALCONY	1,146	1,228	4	4,584	4,912	4	4,584	4,912	4	4,584	4,912	4	4,584	4,912
C1	3BR/2BA BALCONY	1,450	1,564	2	2,900	3,128	2	2,900	3,128	2	2,900	3,128	2	2,900	3,128
C2	3BR/2BA BALCONY	1,338	1,407	4	5,352	5,628	4	5,352	5,628	4	5,352	5,628	4	5,352	5,628
Total:				26	26,416	28,360	26	26,584	28,360	26	26,584	28,360	26	26,584	28,360

Building 3	Total Units:	104
Building 3	Total MKT Net:	106,168
Building 3	Total MKT Gross:	113,440

Project Totals:	Total Units:	302
	Total MKT Net:	307,814
	Total MKT Gross:	328,832

Unit Design	Unit Type	Net	Gross	Clubhouse			Clubhouse			Clubhouse			Clubhouse		
				First			Second			Third			Fourth		
				Qty.	Net	Gross									
PR CLUB	CLUBHOUSE FIRST FLOOR		5,557	1	-	5,557	-	-	-	-	-	-	-	-	
EX CLUB	CLUBHOUSE SECOND FLOOR		4,565	-	-	-	1	-	4,565	-	-	-	-	-	

Clubhouse	Total Units:	3
Clubhouse First	Gross:	5,557
Clubhouse Second	Gross:	4,565
Total Clubhouse	Total Gross:	10,122



A5.0



A5.1

Bella Vista at the National
Amenities

<u>Clubhouse</u>	<u>Size</u>
Business Center	375 SF
Conference Room	300 SF
Message	120 SF
Lounge	450 SF
Open Deck / Cabana	600 SF
Gift Wrapping	155 SF
Entry/Hall	315 SF
Leasing Office	800 SF
Manager Office	10 SF
File/Package Room	148 SF
Break Room	200 SF
Gym	1400 SF
Kid's Room	190 SF
Club Room	1520 SF
Maint. Office	320 SF
IT/Elect.	70 SF
Tanning Room	70 SF
Total:	-

<u>Building 1 Amenity</u>	<u>Size</u>
Seating Area	
Mail Room	
Electrical Room	
Data Closet	
Dog Wash	
Trash Pickup	
Elevator Equip.	
Janitor	
Water	
Total	-

Numbers are for all 4 floor combined

<u>Pool Area</u>	<u>Size</u>
Cabana	
Seat Walls	
Infinity Pool	
Pool Deck/Pool Coping	
Sidewalk	
Firepit	
Kitchen	
Hot Tub	
Grills	
Covered Sitting Area	
Other	

Parking Ratios for Bella Vista at the National

Parkville Standards

1 and 2 Bedroom Apartments	1.5 stalls per unit
3 Bedroom Apartments	2 stalls per unit

Proposed Apartments

		Required
1 and 2 Bedroom Apartments	232	348
3 Bedroom Apartments	70	140
Total		488

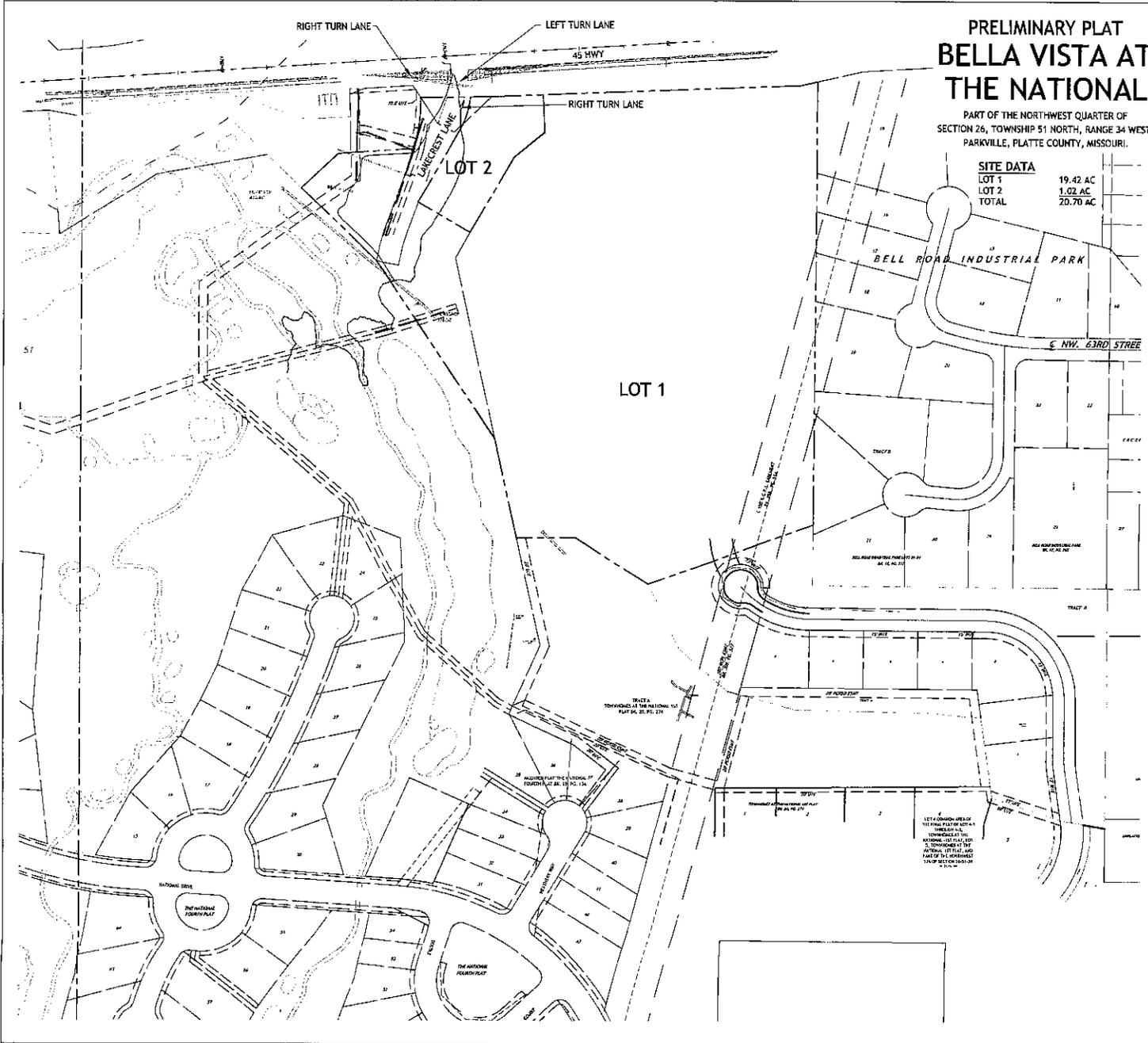
1.63 stalls per unit

Proposed Parking Stalls

Garage in building	240
Garage outside building	46
Surface parking (includes some car ports)	264
Total	550

1.83 stalls per unit

Exceeds required parking by 62 stalls



PRELIMINARY PLAT BELLA VISTA AT THE NATIONAL

PART OF THE NORTHWEST QUARTER OF
SECTION 26, TOWNSHIP 51 NORTH, RANGE 34 WEST,
PARKVILLE, PLATTE COUNTY, MISSOURI.

SITE DATA

LOT 1	19.42 AC
LOT 2	1.02 AC
TOTAL	20.70 AC

PROPERTY DESCRIPTION

TRACT 1
CONTAINING 966.391 SQUARE FEET OR 22.19 ACRES
ALL THAT PART OF TRACT 8, TOWNSHIP 51 NORTH, RANGE 34 WEST AND ALL THAT PART OF THE NORTHWEST QUARTER OF SECTION 26, TOWNSHIP 51 NORTH, RANGE 34 WEST, ALL IN PARKVILLE, PLATTE COUNTY, MISSOURI BEING DESCRIBED AS FOLLOWS: BEGINNING AT THE SOUTHWEST CORNER OF SAID TRACT 8; THENCE N79°28'17"W, ALONG THE SOUTH LINE OF SAID TRACT 8, A DISTANCE OF 65.64 FEET TO A POINT ON THE EAST RIGHT-OF-WAY LINE OF LAKE CREST COURT; THENCE NORTHERLY, ALONG SAID RIGHT-OF-WAY LINE, ALONG A CURVE TO THE LEFT, HAVING AN INITIAL TANGENT BEARING OF N64°22'07"W, A RADIUS OF 225.00 FEET, AN ARC DISTANCE OF 13.93 FEET; THENCE N0°52'34"W, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 111.15 FEET; THENCE NORTHERLY, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, ALONG A CURVE TO THE RIGHT BEING TANGENT TO THE LAST DESCRIBED CURVE, HAVING A RADIUS OF 175.00 FEET, AN ARC DISTANCE OF 34.21 FEET; THENCE N00°00'00"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 133.00 FEET; THENCE NORTHERLY, CONTINUING ALONG SAID RIGHT-OF-WAY LINE AND ALONG THE NORTHERLY RIGHT-OF-WAY LINE OF SAID LAKE CREST COURT, ALONG A CURVE TO THE LEFT BEING TANGENT TO THE LAST DESCRIBED CURVE, HAVING A RADIUS OF 200.00 FEET, AN ARC DISTANCE OF 312.29 FEET; THENCE N0°00'00"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 394.89 FEET; THENCE WESTERLY, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, ALONG A CURVE TO THE RIGHT BEING TANGENT TO THE LAST DESCRIBED CURVE, HAVING A RADIUS OF 75.00 FEET, AN ARC DISTANCE OF 100.07 FEET; THENCE NORTHERLY, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, ALONG A CURVE TO THE RIGHT BEING TANGENT TO THE LAST DESCRIBED CURVE, HAVING A RADIUS OF 19.00 FEET, AN ARC DISTANCE OF 19.75 FEET; THENCE NORTHERLY, WESTERLY, SOUTHERLY, AND EASTERLY, ALONG THE RIGHT-OF-WAY LINE OF SAID LAKE CREST COURT, ALONG A CURVE TO THE LEFT BEING TANGENT TO THE LAST DESCRIBED CURVE, HAVING A RADIUS OF 50.00 FEET, AN ARC DISTANCE OF 244.26 FEET; THENCE EASTERLY, ALONG THE SOUTHERLY RIGHT-OF-WAY LINE OF SAID LAKE CREST COURT, ALONG A CURVE TO THE RIGHT BEING TANGENT TO THE LAST DESCRIBED CURVE, HAVING A RADIUS OF 225.00 FEET, AN ARC DISTANCE OF 124.54 FEET TO A POINT ON THE SOUTHEASTERN LINE OF SAID TRACT 8; THENCE S17°19'17"W, ALONG SAID LINE, A DISTANCE OF 182.27 FEET TO A POINT ON THE SOUTHWEST LINE OF SAID TRACT 8, SAID POINT ALSO BEING A POINT ON THE NORTHERLY LINE OF TRACT A, TOWNSHIP 51 NORTH, RANGE 34 WEST, PLATTE COUNTY, MISSOURI; THENCE N45°40'59"W, ALONG SAID LINE, A DISTANCE OF 76.00 FEET; THENCE N48°27'27"W, CONTINUING ALONG SAID LINE, A DISTANCE OF 145.73 FEET; THENCE N61°23'17"W, CONTINUING ALONG SAID LINE, A DISTANCE OF 146.24 FEET; THENCE N50°18'30"W, CONTINUING ALONG SAID LINE, A DISTANCE OF 180.00 FEET TO THE NORTHWEST CORNER OF SAID TRACT 8, SAID POINT ALSO BEING A CORNER ON THE NORTHERLY LINE OF SAID TRACT A; THENCE N07°23'56"W, ALONG THE NORTH LINE OF SAID TRACT A, A DISTANCE OF 132.90 FEET TO THE NORTHWEST CORNER OF SAID TRACT A; THENCE N12°29'07"W, A DISTANCE OF 222.70 FEET; THENCE N32°53'19"W, A DISTANCE OF 451.35 FEET TO A POINT ON THE SOUTH RIGHT-OF-WAY LINE OF LAKE CREST LANE, AS ESTABLISHED BY THE PLAT OF LAKESIDE COUNTRY CLUB ADDITION, THENCE S70°53'51"E, ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 49.77 FEET TO A POINT ON THE EAST RIGHT-OF-WAY LINE OF SAID LAKE CREST LANE; THENCE N08°08'07"E, ALONG SAID RIGHT-OF-WAY LINE, ALONG A CURVE TO THE RIGHT, HAVING AN INITIAL TANGENT BEARING OF N09°22'27"E, A RADIUS OF 294.38 FEET, AN ARC DISTANCE OF 72.14 FEET; THENCE S00°12'17"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 123.46 FEET; THENCE N17°29'07"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 93.25 FEET; THENCE NORTHERLY, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, ALONG A CURVE TO THE LEFT, HAVING AN INITIAL TANGENT BEARING OF N12°16'27"E, A RADIUS OF 124.64 FEET, AN ARC DISTANCE OF 34.92 FEET TO A POINT ON THE SOUTHERLY RIGHT-OF-WAY LINE OF MISSOURI ROUTE NO. 45; THENCE S48°30'36"E, ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 22.21 FEET; THENCE N0°00'00"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 114.19 FEET; THENCE N07°46'52"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 60.91 FEET; THENCE N09°55'27"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 38.39 FEET TO A POINT ON THE WEST LINE OF BELL ROAD INDUSTRIAL PARK, A SUBDIVISION IN SAID PARKVILLE, MISSOURI; THENCE S00°27'04"W, ALONG THE WEST LINE OF SAID BELL ROAD INDUSTRIAL PARK, A DISTANCE OF 181.45 FEET TO THE SOUTHWEST CORNER OF SAID BELL ROAD INDUSTRIAL PARK, SAID POINT ALSO BEING ON THE NORTH LINE OF TRACT 8, SAID TOWNSHIP 51 NORTH, RANGE 34 WEST, PLATTE COUNTY, MISSOURI; THENCE S89°22'53"E, ALONG THE SOUTH LINE OF SAID BELL ROAD INDUSTRIAL PARK AND ALONG THE NORTH LINE OF SAID TRACT 8, A DISTANCE OF 408.17 FEET TO THE NORTHWEST CORNER OF SAID TRACT 8, SAID POINT ALSO BEING THE NORTHWEST CORNER OF BELL ROAD HARBOR, A SUBDIVISION IN SAID PARKVILLE; THENCE S00°26'36"W, ALONG THE EAST LINE OF SAID TRACT 8 AND THE WEST LINE OF SAID BELL ROAD HARBOR, A DISTANCE OF 537.47 FEET TO THE POINT OF BEGINNING.

TRACT 2
CONTAINING 176.939 SQUARE FEET OR 4.08 ACRES
ALL THAT PART OF THE NORTHWEST QUARTER OF SECTION 26 AND PART OF THE NORTHWEST QUARTER OF SECTION 22, TOWNSHIP 51 NORTH, RANGE 34 WEST, PARKVILLE, PLATTE COUNTY, MISSOURI BEING DESCRIBED AS FOLLOWS: BEGINNING AT THE SOUTHWEST CORNER OF LOT 1, CITY PLAZA, A SUBDIVISION IN SAID PARKVILLE; THENCE N86°21'56"E, ALONG THE SOUTH LINE OF SAID TOFFI PLAZA, A DISTANCE OF 108.88 FEET TO THE SOUTHWEST CORNER OF SAID TOFFI PLAZA, SAID POINT ALSO BEING ON THE WEST RIGHT-OF-WAY LINE OF LAKE CREST LANE; THENCE S17°29'07"W, ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 58.83 FEET; THENCE S07°17'00"W, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 122.36 FEET; THENCE SOUTHERLY, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, ALONG A CURVE TO THE LEFT BEING TANGENT TO THE LAST DESCRIBED CURVE, HAVING A RADIUS OF 424.76 FEET, AN ARC DISTANCE OF 814.62 FEET; THENCE N06°12'07"W, A DISTANCE OF 40.90 FEET; THENCE S01°46'28"W, A DISTANCE OF 441.39 FEET; THENCE N03°28'42"W, A DISTANCE OF 222.80 FEET TO A POINT ON THE SOUTHERLY RIGHT-OF-WAY LINE OF MISSOURI ROUTE NO. 45; THENCE S50°22'47"E, ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 63.73 FEET; THENCE N01°50'07"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 242.84 FEET; THENCE N06°22'12"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 141.02 FEET; THENCE S41°29'42"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 58.08 FEET; THENCE S73°07'37"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 66.12 FEET; THENCE N07°16'07"E, CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 89.79 FEET TO A POINT ON THE WEST LINE OF SAID TOFFI PLAZA; THENCE S07°50'37"E, ALONG SAID WEST LINE, A DISTANCE OF 78.02 FEET TO THE POINT OF BEGINNING.

A NEW MULTIFAMILY DEVELOPMENT FOR:
Bella Vista at the National

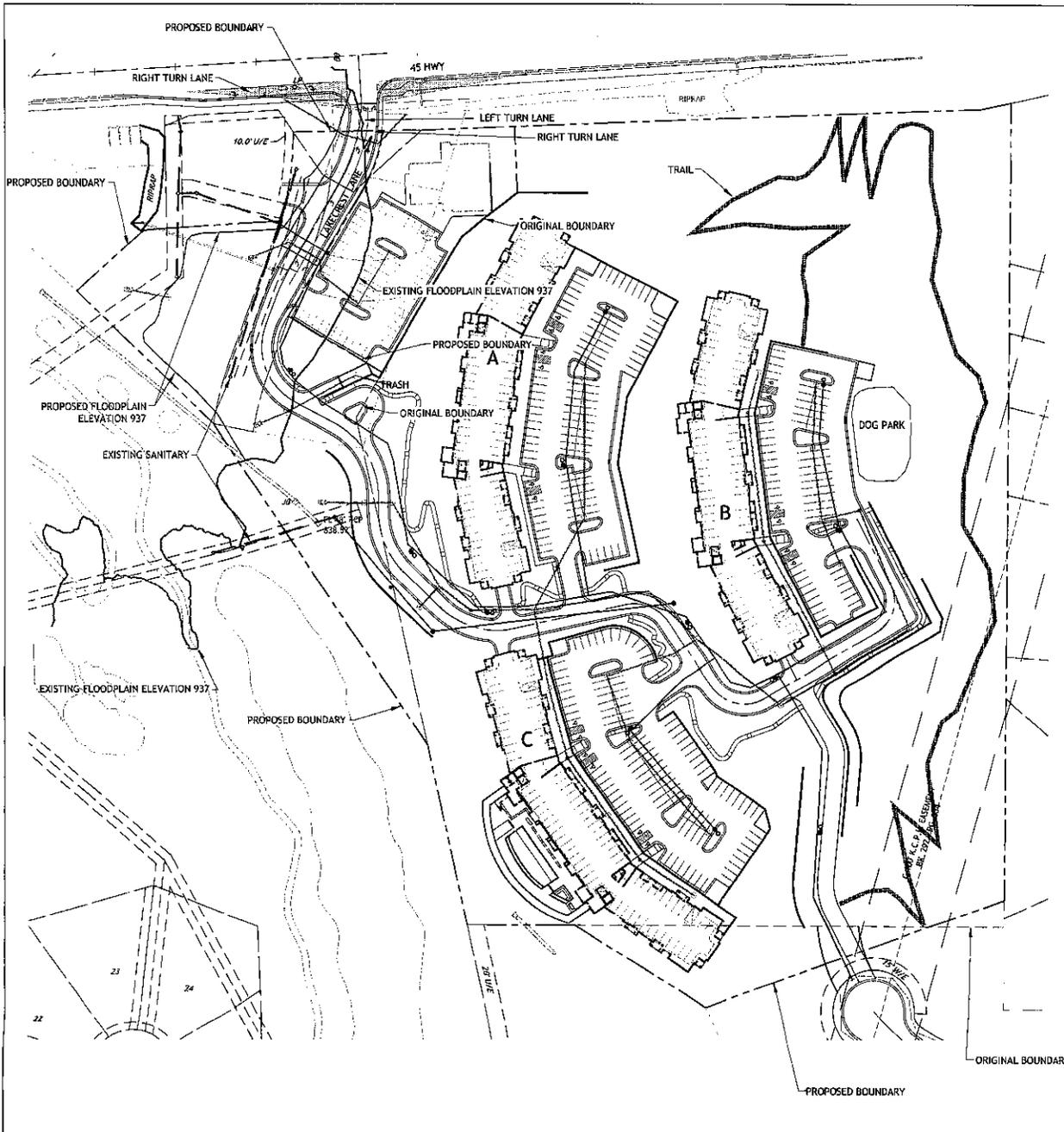
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REVISIONS:
1-1-17 CITY COMMENTS

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Steve@wargerassociates.com

PROJECT: _____
DATE: _____
SHEET NUMBER: _____
C1



PRELIMINARY DEVELOPMENT PLAN BELLA VISTA AT THE NATIONAL

PART OF THE NORTHWEST QUARTER OF
SECTION 26, TOWNSHIP 51 NORTH, RANGE 34 WEST,
PARKVILLE, PLATTE COUNTY, MISSOURI.

APARTMENT PARKING COUNT

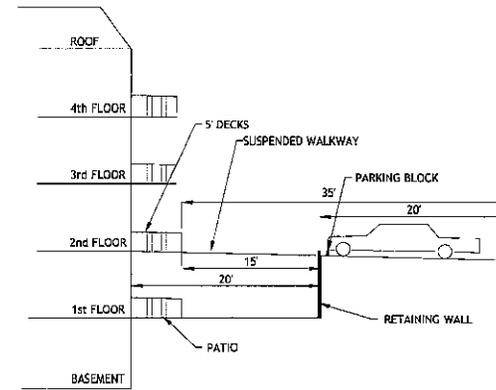
BUILDING	UNDER BUILDING	OUTSIDE	OUTSIDE GARAGE	TOTAL
A	80	94	19	193
B	80	76	0	156
C	80	94	27	201
TOTAL				550 =
				1.83 STALLS / UNIT

LOT 2 PARKING COUNT

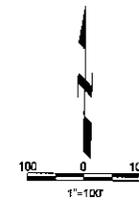
1-STORY BUILDING	7,000SF
PARKING	67 = 9.57 STALL / 1000sf

SITE DATA

TOTAL SITE	30.32 AC
LOT 1	19.42 AC
LOT 2	1.02 AC
PUBLIC R/W	0.26 AC
TOTAL	20.70 AC
OPEN SPACE	901,692 SF (20.53AC) - IMPERVIOUS AREA 263,973 SF = 637,719 SF = 70.72%



SECTION FOR BUILDINGS B AND C



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A NEW MULTI-FAMILY DEVELOPMENT FOR:
Bella Vista at the National

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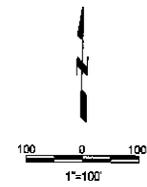
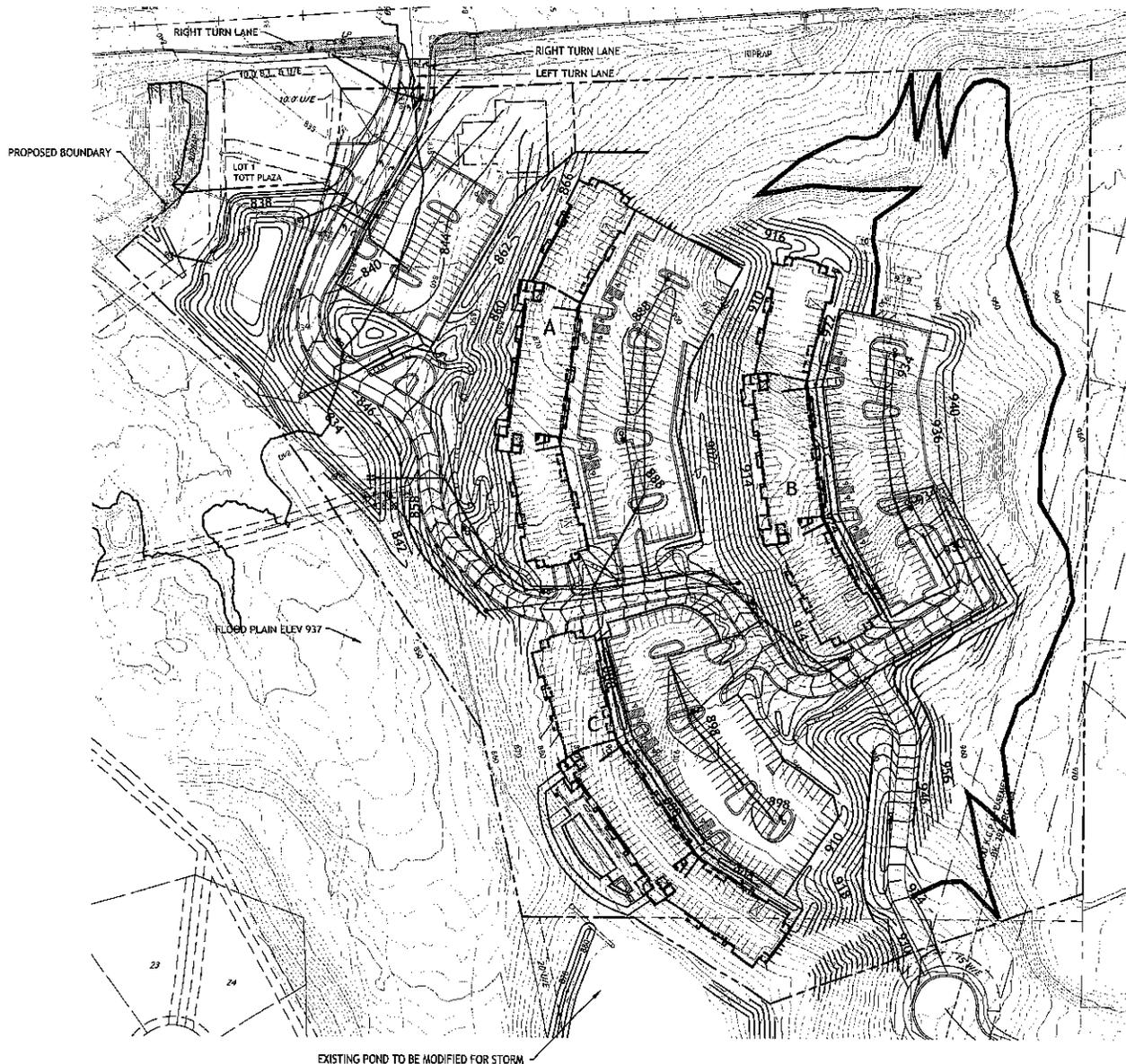
REVISIONS:
6-5-16 CITY COMMENTS

SITE PLAN

PROJECT: DATE: SCALE:
SHEET NUMBER: C-2

PRELIMINARY PLAN BELLA VISTA AT THE NATIONAL

PART OF THE NORTHWEST QUARTER OF
SECTION 26, TOWNSHIP 51 NORTH, RANGE 34 WEST,
PARKVILLE, PLATTE COUNTY, MISSOURI.



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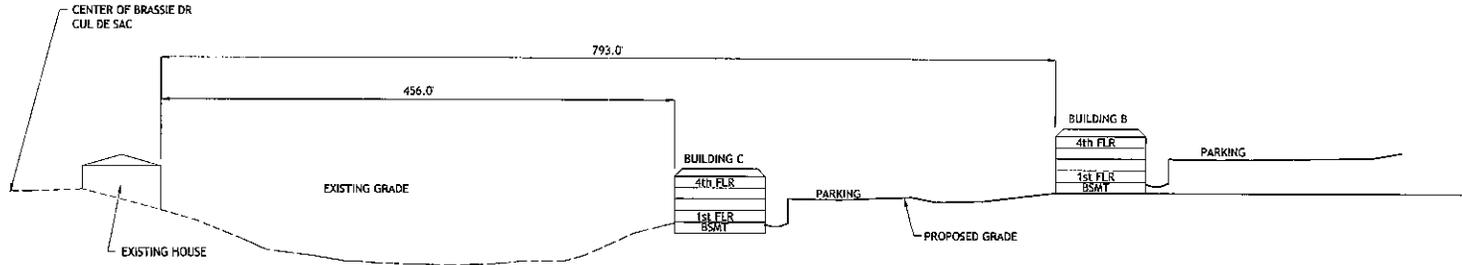
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GRADING PLAN

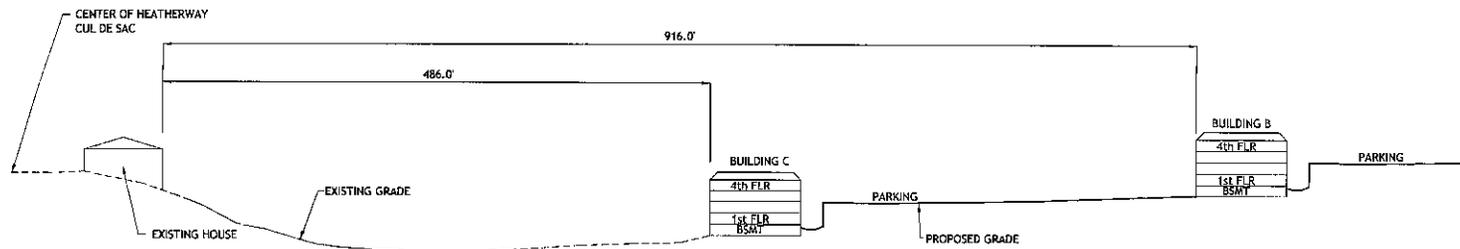
PROJECT: THE NATIONAL
SHEET NUMBER: C-3

PRELIMINARY PLAN BELLA VISTA AT THE NATIONAL

PART OF THE NORTHWEST QUARTER OF
SECTION 26, TOWNSHIP 51 NORTH, RANGE 34 WEST,
PARKVILLE, PLATTE COUNTY, MISSOURI.



NORTH SITE SECTION FROM
BRASSIE DR CUL DE SAC



SOUTH SITE SECTION FROM
HEATHERWAY CUL DE SAC

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REVISIONS	CITY COMMENTS
6-4-15	

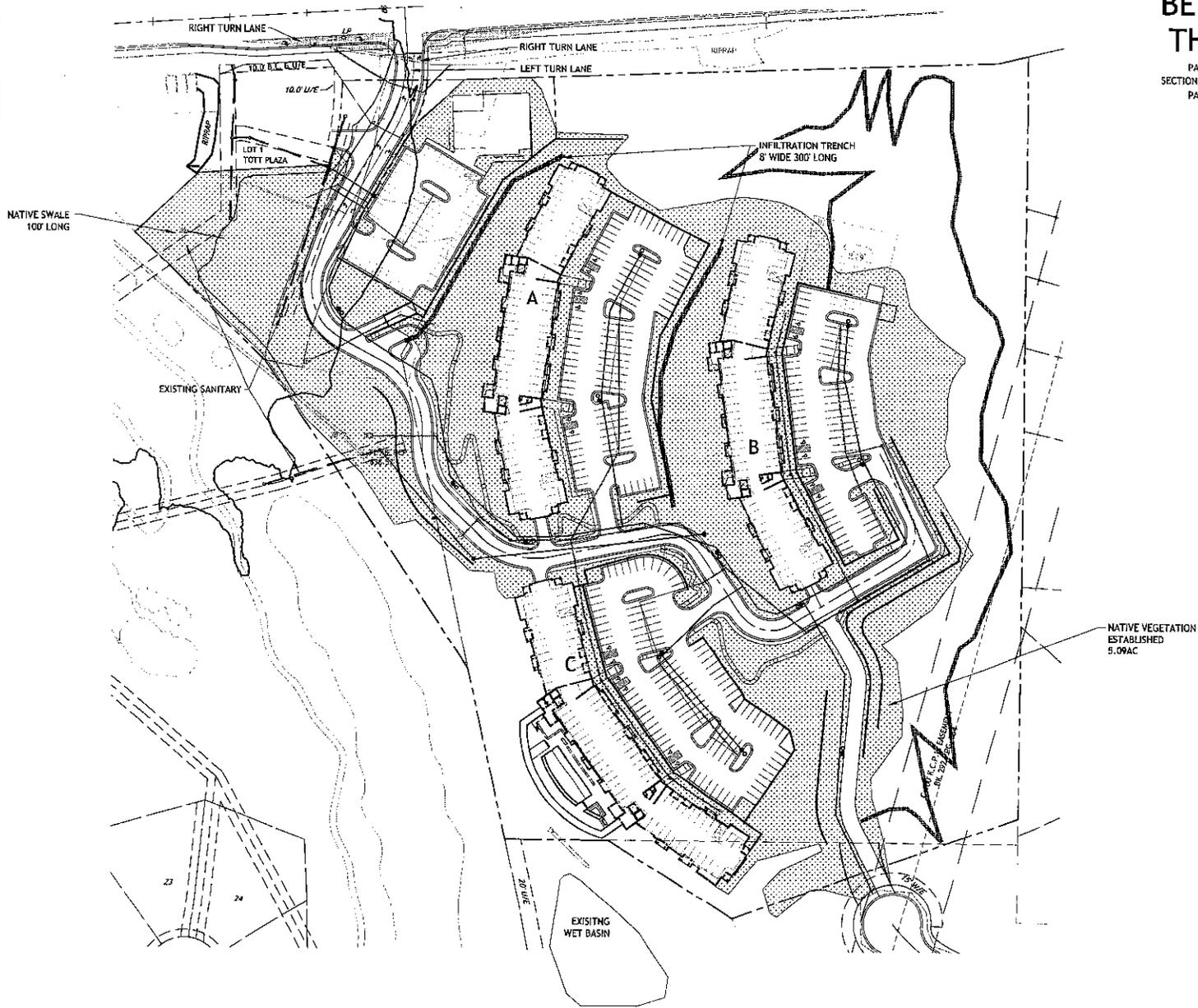


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SITE SECTIONS	DATE	C-4
	SHEET NUMBER	

PRELIMINARY PLAN BELLA VISTA AT THE NATIONAL

PART OF THE NORTHWEST QUARTER OF
SECTION 26, TOWNSHIP 51 NORTH, RANGE 34 WEST,
PARKVILLE, PLATTE COUNTY, MISSOURI.



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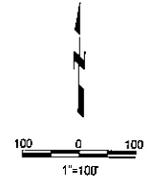


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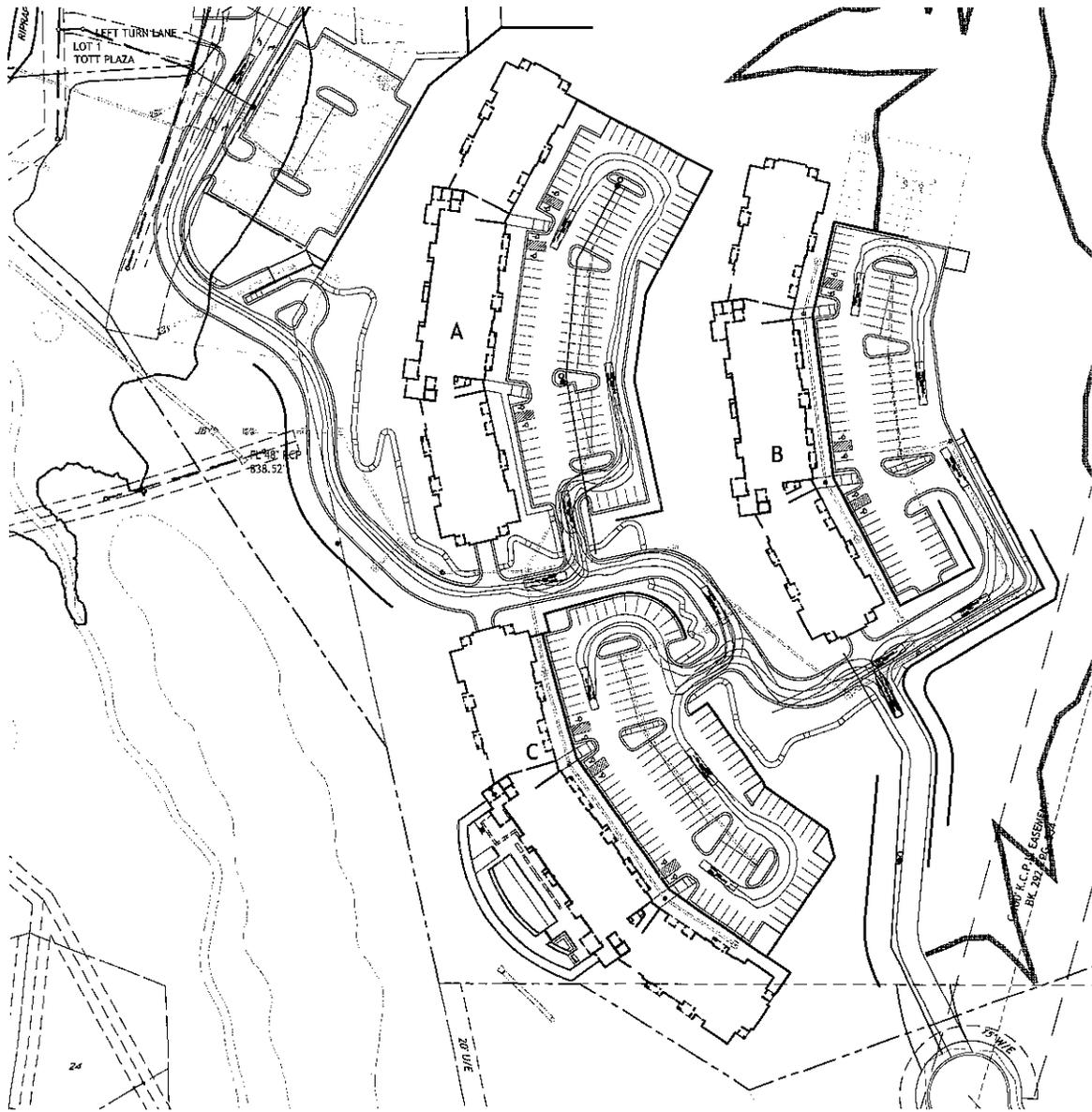
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5-6-15 CITY COMMENTS



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SHEET NUMBER	DATE	BY	CHECKED	SCALE	PROJECT	SHEET NUMBER



PRELIMINARY PLAN BELLA VISTA AT THE NATIONAL

PART OF THE NORTHWEST QUARTER OF
SECTION 26, TOWNSHIP 51 NORTH, RANGE 34 WEST,
PARKVILLE, PLATTE COUNTY, MISSOURI.

A NEW MULTI-FAMILY DEVELOPMENT FOR:
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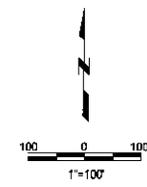


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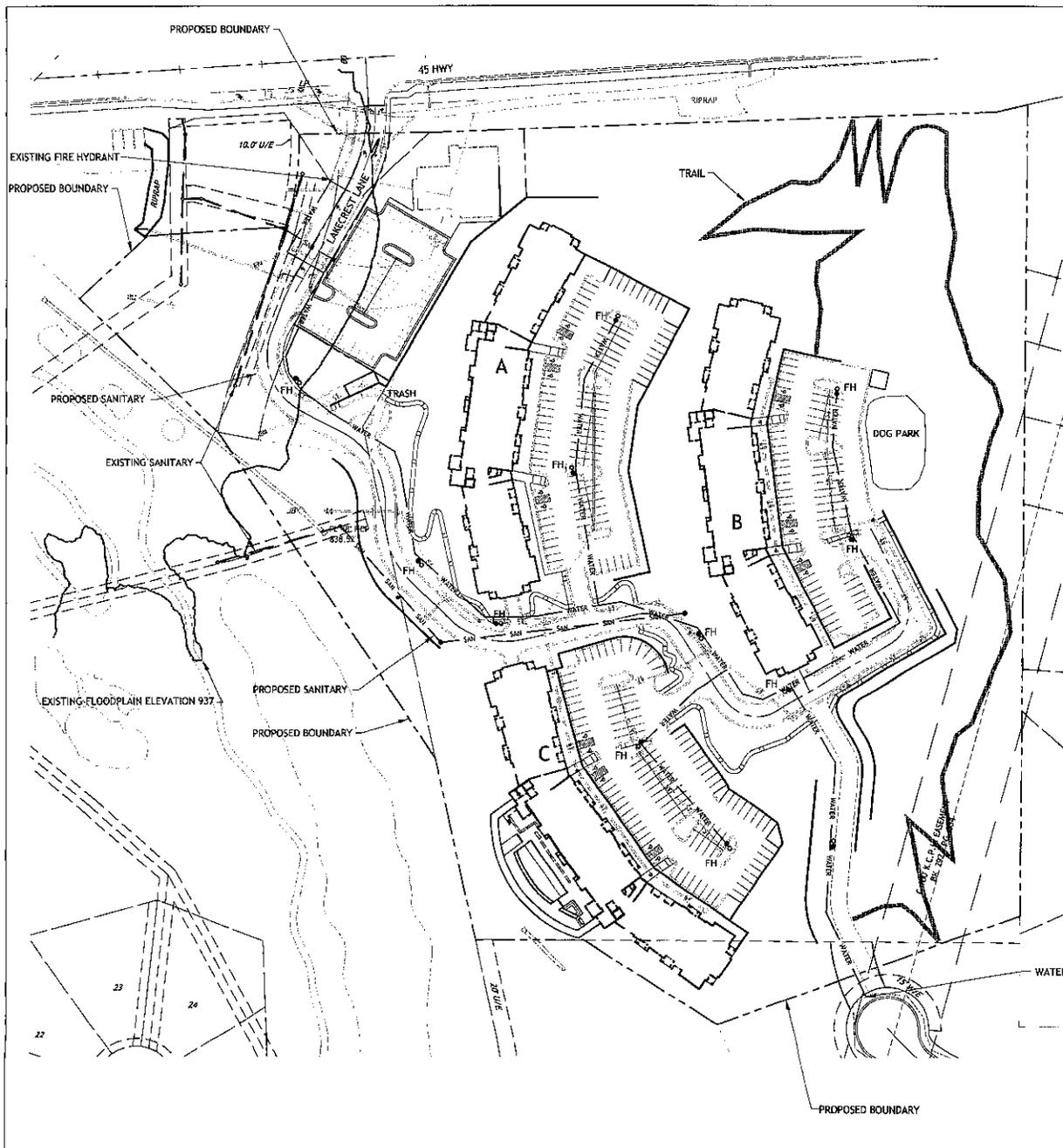
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6-6-16 CITY COMMENTS



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FIRE ACCESS
PROJECT: DANIE
DATE: 6/6/16
DRAWN BY: SLEVE
CHECKED BY: SLEVE
C-6



PRELIMINARY DEVELOPMENT PLAN BELLA VISTA AT THE NATIONAL

PART OF THE NORTHWEST QUARTER OF
SECTION 26, TOWNSHIP 51 NORTH, RANGE 34 WEST,
PARKVILLE, PLATTE COUNTY, MISSOURI.

LEGEND
FH = FIRE HYDRANT

A NEW MULTI-FAMILY DEVELOPMENT FOR
Bella Vista at the National

M/O: Highway No. 45 & Lake Crest Lane
P. 10/14/15
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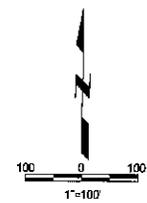
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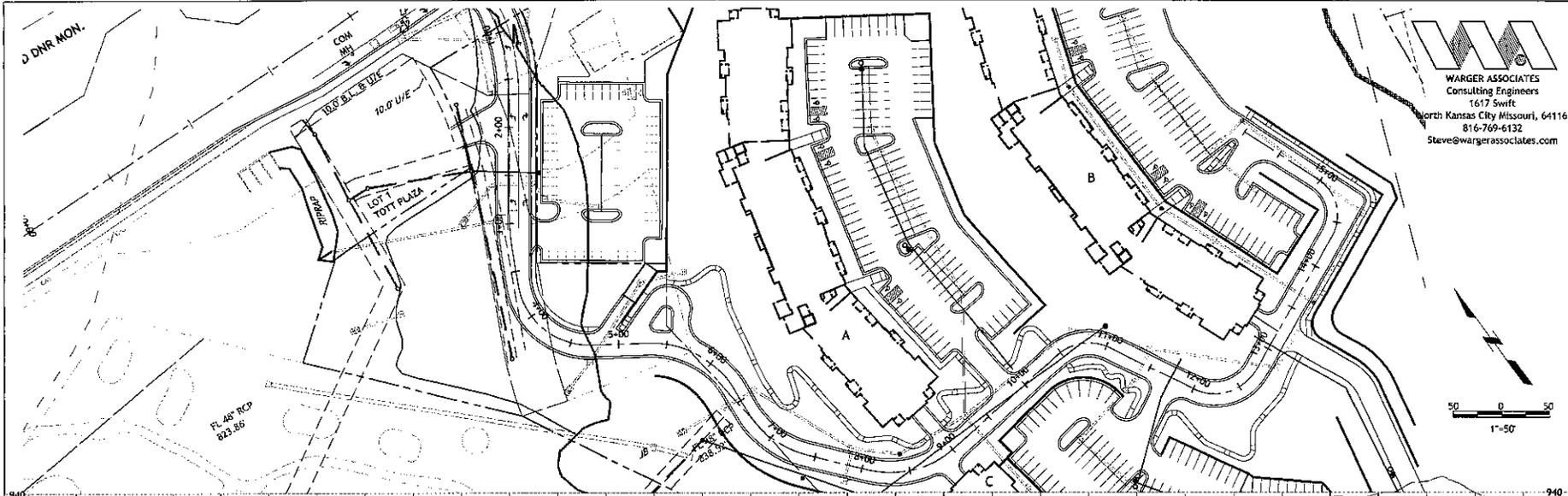
UTILITY PLAN

PROJECT: DATE: 12-11-15
SCALE: SHEET NUMBER:
C-7

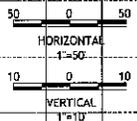
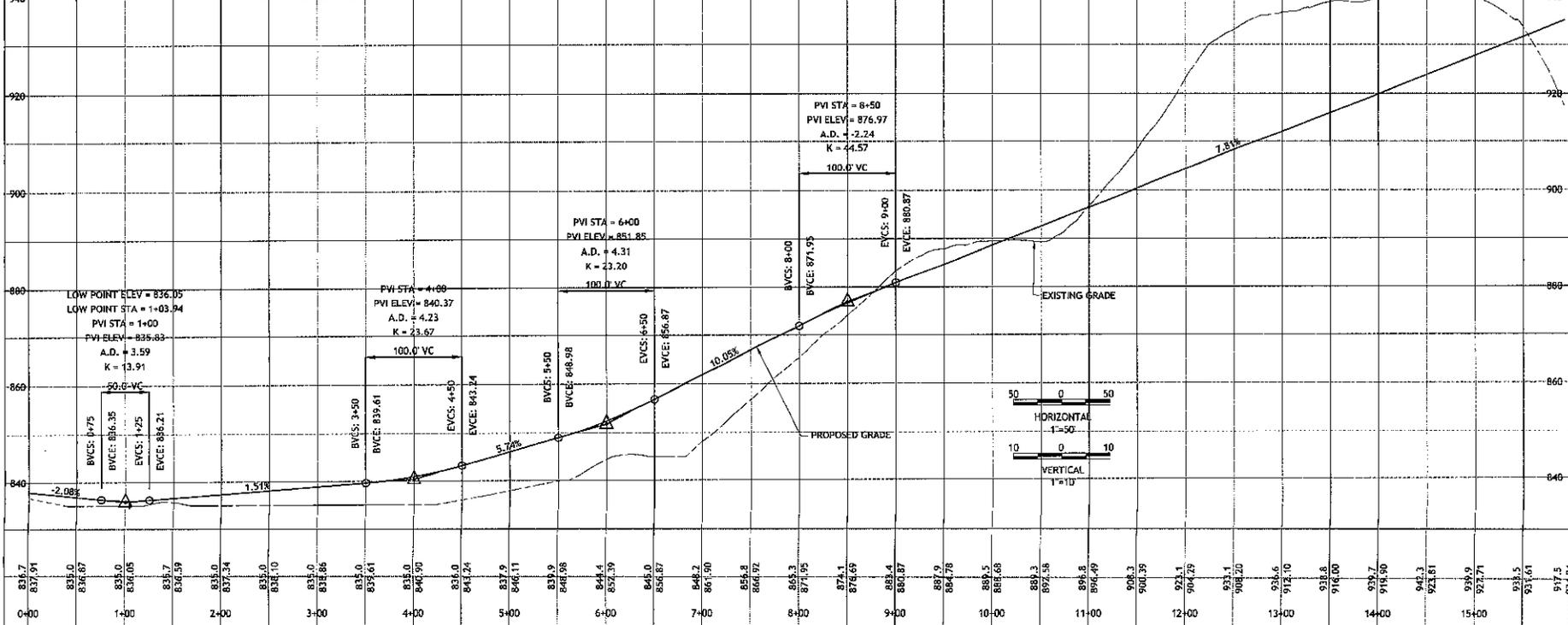
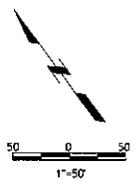


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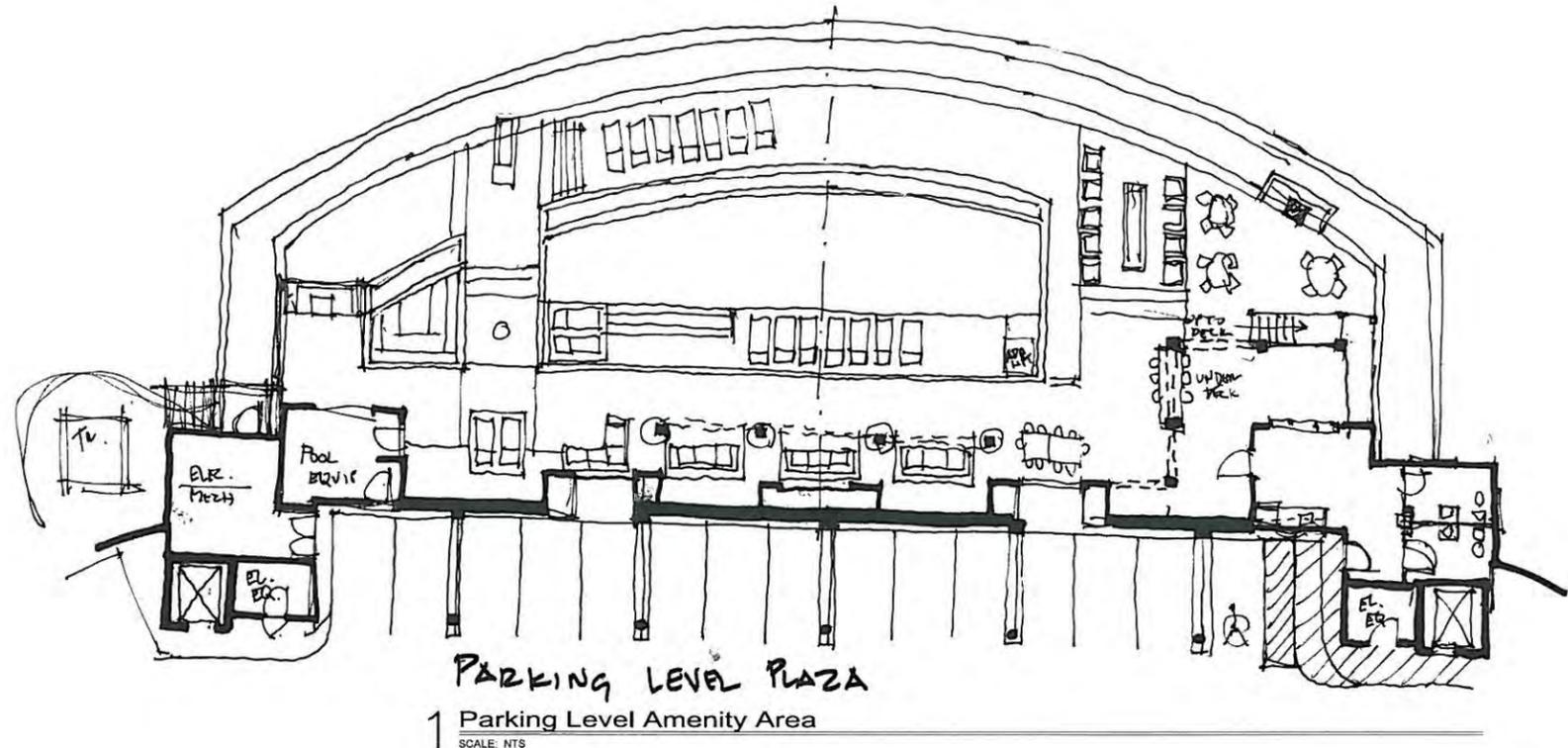

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A NEW MULTI-FAMILY DEVELOPMENT FOR:
Bella Vista at the National
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MAIN ROAD
PLAN & PROFILE
 PROJECT: DATE: 11/15/15
 SHEET NUMBER: C-8



PARKING LEVEL PLAZA

1 Parking Level Amenity Area
SCALE: NTS



2 Building Side Elevation
SCALE: NTS

PRELIMINARY DEVELOPMENT PLAN

A NEW MULTI-FAMILY DEVELOPMENT FOR:
Bella Vista at the National

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REVISIONS

NO.	DATE	DESCRIPTION

PROJECT:	DATE:
NO.:	APRIL 2, 2015
SHEET NUMBER:	

Bella Vista at the National

SCHEDULE OF OPEN HOURS FOR AMENTITIES

Office hours:	9am-6 pm
Pool:	9 am-9 pm
Club house/gathering area:	9 am-9 pm
Fitness center:	24 hours
Dog park:	24 hours
Maintenance area:	7am-10 pm
Car Care Area:	7am 10pm

Special events at Clubhouse: by reservation 1am is latest end time allowed

**CITY OF PARKVILLE
INTRACITY COMMUNICATION**

MAY 8, 2015

TO: SEAN ACKERSON
FROM: ALYSEN ABEL

RE: PLAN REVIEW COMMENTS – MAY 12, 2015 PC MEETING

BELLA VISTA AT THE NATIONAL
PUBLIC WORKS COMMENTS

Stormwater

The applicant submitted a preliminary stormwater management study that evaluated the stormwater issues associated with the site. The City has reviewed and approved the preliminary stormwater study. A final stormwater study should be submitted at the time of construction plan submittal containing final design calculations.

The applicant's engineer evaluated the need for stormwater detention. The calculations were based on APWA 5600 design standards and applicable City codes. The applicant will provide the necessary detention volume with the reconstruction of the existing detention pond adjacent to the site. Final calculations and design details associated with the detention area will need to be submitted at the time of construction plan submittal.

The applicant's engineer evaluated the need for stormwater treatment. The calculations were based on the APWA BMP Manual and applicable City codes. The applicant will provide the necessary water quality in (1) native vegetation; (2) native vegetated swales; (3) an infiltration trenches; and (4) an extended wet detention pond. Final calculations and design details associated with the stormwater treatment facilities will need to be submitted at the time of construction plan submittal.

Traffic

The developer's engineer (Priority Engineers, Inc.) prepared a Traffic Impact Study for the Bella Vista at the National apartments. The City contracted with TranSystems to perform a peer review of the Traffic Impact Study. The traffic study included the anticipated volumes associated with the proposed development for the Existing, Existing plus Development Conditions, and Future Conditions.

The site plan included a 12-foot access road from the development that extends to the south to Limestone Road in the Townhomes at the National development. The purpose of this access road is to provide a second point of access for the fire department. The developer will install a gate at the entrance and cannot be accessed by the public.

The developer has expressed an interest in making the majority of Lake Crest Drive a private drive. The developer would be responsible for the long-term maintenance. The City would have to go through the formal process of vacating the right-of-way. The City recommends either terminating the public right-of-way at the south end of the commercial entrances, or providing an access easement for public use of the private drive.

The study recommends expanding the entrance off Hwy 45 to include three lanes, (1) inbound lane, (2) left turn lane, and (3) thru/right lane. The study also recommends the construction of an eastbound right turn lane on Hwy 45, with 120 feet of storage and a 100-foot taper.

The intersection of Hwy 45 and Lake Crest Lane was reviewed in the traffic study. With the construction of the 300 unit apartment complex, a traffic signal may be warranted during the peak hours. The traffic study included recommendations associated with the traffic signal, including the following considerations.

- Shifting the transition of the speed limit on Hwy 45 from 45 mph to 35 mph from the west side of Graden Road to the west side of the Lake Crest Lane intersection; or
- Installing a traffic signal at the intersection of Hwy 45 and Lake Crest Lane after the construction of Building 2, but prior to the construction of Building 3.

Erosion Control

The City will review the erosion and sediment control practices at the time of construction plan review. Additional measures will be necessary to ensure that sediment does not leave the site. Practices such as a sediment basin or sediment trap may be evaluated at the time of construction plan review, in addition to other erosion and sediment controls in accordance with the APWA 5100 design standards. Turf reinforcement mat shall be placed on any disturbed areas with slopes of 3:1 or more.

The Public Works staff can recommend approval with the following conditions:

- a. Concurrent with the submittal of construction plans, the developer's engineer shall provide a Final Stormwater Management Study that contains final design calculations for the stormwater system. The study shall be approved prior to issuance of a permit.
- b. The applicant's engineer shall submit detailed drawings and engineering calculations associated with the stormwater detention and stormwater treatment facilities. Drawings and calculations shall be approved prior to issuance of a grading permit.
- c. At the time of construction plan review, the Public Works staff will review the need for additional agreements, easements, and bonds associated with the construction of the stormwater detention and stormwater treatment facilities.
- d. The developer shall install a private access from the southern edge of their property to Limestone Court, for use by the fire department. Gates shall be installed at either end of the private drive to prevent public access.
- e. Upon vacation of the public right-of-way on Lake Crest Lane, the terminus of the public right-of-way shall be south of the commercial entrances, or the developer shall provide an access easement for public use of the private drive.
- f. The applicant shall provide the following improvements in accordance with the traffic study:
 1. Expand the Lake Crest Lane entrance off of Hwy 45 to a three lane configuration with one inbound lane and two outbound lanes.
 2. Provide an eastbound right turn lane with 120 feet of lane storage and a 100-foot taper.
 3. Either convert the speed limit to a lower speed west of the Lake Crest Lane entrance or assist with the financing of a traffic signal at the intersection at Lake Crest Lane, contingent on approval of MoDOT.

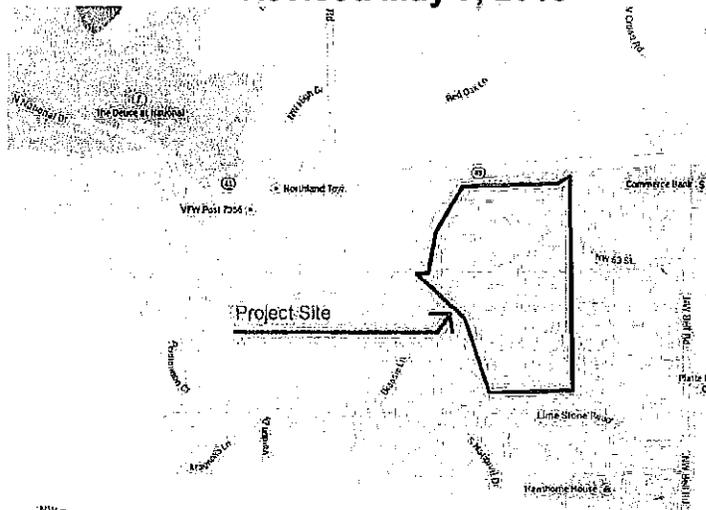
- g. The developer's engineer shall provide erosion and sediment control plans in accordance with APWA 5100 design standards. Additional measures may be deemed necessary by City staff upon review. Practices such as a sediment basin or sediment trap may be evaluated at the time of construction plan review. Turf reinforcement mat shall be placed on any slopes of 3:1 or greater.
- h. Structural calculations are necessary for the retaining walls on site. The developer's engineer shall submit structural calculations concurrent with the construction plan submittal.

Bella Vista - Parking Calculations

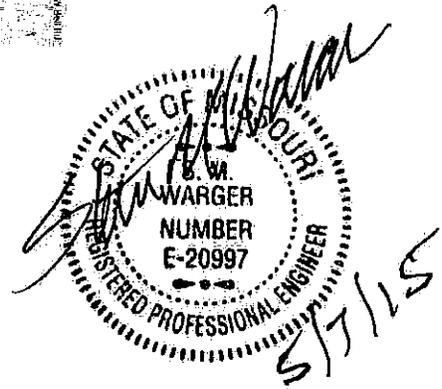
# of bedrooms	Unit type	# of units	Spaces per	Total Spaces	Spaces / Unit
1					
	A1	48			
	A2	28			
	A2S	17			
	A3	24			
		<u>117</u>	1.5	<u>175.5</u>	
2					
	B1	68			
	B1S	0			
	B2	47			
		<u>115</u>	1.5	<u>172.5</u>	
3					
	C1	22			
	C2	48			
		<u>70</u>	2	<u>140</u>	
Total		<u><u>302</u></u>		<u><u>488</u></u>	1.62
Provided				550	1.82

MACRO-STORMWATER DRAINAGE STUDY BELLA VISTA MISSOURI RIVER WATERSHED

SE of Intersection of Hwy 45 and Lake Crest Lane
Parkville, MO 64152
Section 26 Township 51N Range 34W
March 31, 2015
Revised May 7, 2015



Prepared for:
Bella Vista at the National
Adam Tholen



Warger Associates LLC
Consulting Engineers
1617 Swift
N. Kansas City, MO. 64116
816-769-6132

WA No. 385-01

PARKVILLE PUBLIC WORKS DEPT.

Approved for:

Preliminary Development Plan

Date: 5/8/15

By: AMA

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BIBLIOGRAPHY

CITY OF PARKVILLE, PLATTE COUNTY, MISSOURI CITY CODE, LATEST EDITION – CHAPTER 507 –

<http://www.ecode360.com/27903218>

APWA 5600 SPECIFICATIONS –

<http://kcmetro.apwa.net/chapters/kcmetro/specs/APWA5600.pdf>

APWA 5600 SUPPLEMENT(S) -

http://www.kcmo.org/idc/groups/publicworks/documents/publicworks/specifications_apwa5600supp1.pdf

APWA DRAINAGE BMP MANUAL –

<https://data.kcmo.org/Land-Development/BMP-Manual-APWA-03-2008/y8s3-kjlx>

GEOTECHNICAL REPORT –

Report of Geotechnical Exploration, Bella Vista, Parkville, Latest Edition.

GOOGLE MAP –

<https://maps.google.com/maps?hl=en>

FEMA MAP SERVICE CENTER -

<https://msc.fema.gov/webapp/wcs/stores/servlet/CategoryDisplay?catalogId=10001&storeId=10001&categoryId=12001&langId=-1&userType=G&type=1&dfirmCatId=12009&future=false>

UNITED STATES OF AGRICULTURE – NATURAL RESOURCES CONSERVATION SERVICE -

<http://websoilsurvey.nrcs.usda.gov/app/WebSoilSurvey.aspx>

SOFTWARE PACKAGE -

Bentley Systems, Inc. Haestad Methods Solution Center, Bentley PondPack V8i

GENERAL INFORMATION

This storm drainage study is being submitted on behalf of Bella Vista at the National for the proposed multi-family residential development located SE of Intersection of Hwy 45 and Lake Crest Lane in Section 26 Township 51N Range 34W, Parkville, Platte County, Missouri which is in the Missouri River Watershed (See Figure 1 of Appendix A for site plan). The purpose of this study is to provide recommendations to ensure that storm water runoff from the proposed development will not have an adverse impact on existing downstream developments. This storm drainage study will examine the existing and proposed conditions at the above referenced site and determine if the proposed improvement adheres to all local, state, and federal requirements including but not limited to the City of Parkville, Missouri Storm Water requirements.

The proposed development consists of approximately 13.62 acres of land disturbance on the 24.87 acre property. The site is bordered by partially developed residential land to the south, public right of way for Highway 45 to the north, commercial property to the east, and a private golf course to the west. The area being studied is currently undeveloped. The project is scheduled to consist of public infrastructure and private development for multi-family residential buildings.

The project site has three (3) major soil types according to the Natural Conservation Service (NRCS) Web Soil Survey and is listed below. The full soils report can be found in Appendix B.

- Knox silty clay loam, 9 to 14 percent slopes, severely eroded
- Snead-Rock outcrop complex, 14 to 30 percent slopes
- Kennebec silt loam, 1 to 4 percent slopes, frequently flooded

METHODOLOGY

This report was prepared in accordance with the provisions of the City of Parkville, Platte County, Missouri City Code, Latest Edition – Chapter 507. The analytical and design criteria used in the study conform to those of "Division V - Section 5600 – Storm Drainage Systems and Facilities" of the American Public Works Association's "Standard Specifications and Design Criteria" dated February 2006 and all supplements to the APWA Section 5600. Based on these criteria, allowable discharge from the development is based on limiting 100-year (1%), and 10-year (10%) post development discharges to no more than existing discharges from the site for each respective storm.

Stormwater discharges from the site for the existing and proposed watersheds were evaluated using the SCS Technical Release No. 55 per APWA Section 5602.2. Existing times of concentration were determined using Inlet Time and Travel Time equations found in section 5602.7 of APWA Section 5600. A minimum inlet time of five minutes was used when calculated times were under five minutes. Proposed times of concentration were calculated in the same manner.

FEMA FLOOD CLASSIFICATION

The existing site is located outside of the 100-year flood plain. This area lies in Zone X according to the Flood Insurance Rate Map, FIRM 29165C0379C, Map Revised Preliminary September 30, 2010. See Appendix C for a copy of the FIRM. The FIRM identifies Zone X as "Areas determined to be outside the 0.2% annual chance floodplain."

EXISTING CONDITIONS

The site consists of property located SE of Intersection of Hwy 45 and Lake Crest Lane in Parkville, Missouri. The proposed disturbed area is located outside of the 100 year flood plain per the FIRM Map. The proposed development routes runoff northwesterly to Walnut Creek. Existing drainage boundaries can be found in Figure 1 of Appendix A. The site is comprised of mostly woods and a small area of brush, weeds, and grass adjacent to Walnut Creek in fair condition in the B and D hydrologic groups. There is existing single family residential that is routed through the site and is included within the design. The existing drainage basin results in an overall curve number of 77. Runoff to the northwest is routed overland and through existing 48" RCP culverts. Existing drainage boundaries can be found in Figure 2 of Appendix A. A brief summary of existing runoff can be found in Table 1.

Table 1: Existing Drainage Calculations

Drainage Basin	Area, Acres	Curve Number	Q-2 Year, cfs	Q-5 Year, cfs	Q-10 Year, cfs	Q-25 Year, cfs	Q-50 year, cfs	Q-100 Year, cfs
Bella Vista	58.69	77	107.64	156.13	219.85	278.89	312.10	379.06
Total	58.69	77	107.64	156.13	219.85	278.89	312.10	379.06

PROPOSED CONDITIONS

The proposed project will include the construction of public infrastructure, private roads, public and private utilities, and landscaping. A portion of the property will convey the 100-year design runoff within a private storm sewer system and offsite area upstream of an existing pond which will be modified to a wet detention basin and the remainder will be allowed to be released un-detained. The proposed drainage patterns will vary from the existing drainage boundaries in that more area will be routed to the detention basins which will allow us to over-detain in these drainage basins while allowing the remaining drainage basins to release un-detained. The proposed development and upstream development results in an overall curve number of 81. The site is located outside of the 100-year flood plain for Walnut Creek. Proposed drainage boundaries can be found in Figure 3 of Appendix A. A brief summary of proposed runoff can be found in Table 2.

Table 2: Proposed Drainage Calculations

Drainage Basin	Area, Acres	Curve Number	Q-2 Year, cfs	Q-5 Year, cfs	Q-10 Year, cfs	Q-25 Year, cfs	Q-50 year, cfs	Q-100 Year, cfs
Bella Vista	58.69	81	135.45	187.59	254.13	314.99	348.97	563.10
Total	58.69	81	135.45	187.59	254.13	314.99	348.97	563.10

DETENTION ANALYSIS

The proposed project does increase the runoff from the site and the subject site lies above the Missouri River flood boundaries. Therefore, it is our recommendation that detention should be provided on the site. We are proposing that a portion of the site that was not disturbed on the north, west, and middle be routed un-detained, while the remainder of the site and offsite area be routed through an existing pond that is to be modified into a detention basin. Runoff will be routed to the basin within a private storm sewer system and overland for the 100 year event. A brief summary of the proposed detention can be found in Table 3.

Table 3: Summary of South Detention Basin Design

Bella Vista	Q-2	Q-5	Q-10	Q-25	Q-50	Q-100
Drainage Area	58.69 acres					
Curve Number	81					
Detained Discharge	70.46 cfs	98.71 cfs	139.45 cfs	172.80 cfs	191.57 cfs	234.93 cfs
Detained Peak Hours	12.10	12.05	12.05	12.05	12.05	12.05
Storage Volume	0.575	0.706	0.798	0.869	0.913	0.987
Storage Elevation	866.15	866.85	867.29	867.61	867.82	868.46
Basin Flowline	864.00					
Outlet Structure	1-2'x5' Area Orifice @ 864.00 6'x6' Riser/Weir @ 866.50					
100-Year Emergency Weir Elevation	869.00, Crest Height = 2.0', Length = 138.42'					
Basin Top Elevation	872.00					

The detention basin 100-year emergency weir elevation has the 0.50' freeboard from the 100-year peak elevation, and the 1.00' freeboard from the 100-year crest elevation to the top of the detention basin.

Table 4 below summarizes the comparison between the existing total runoff and proposed total runoff for each sub-basin for the 2-year, 5-year, 10-year, 25-year, 50-year, and 100-year storm events.

Table 4: Comparison of Existing and Proposed Peak Flow Release Rate from the Site

Drainage Basin	Q-2 Year, cfs	Q-5 Year, cfs	Q-10 Year, cfs	Q-25 Year, cfs	Q-50 Year, cfs	Q-100 Year, cfs
Existing Bella Vista	107.64	156.13	219.85	278.89	312.10	379.06
Proposed Bella Vista*	107.94	147.97	219.40	277.73	306.76	366.32

*Includes undetained runoff

BMP ANALYSIS

Worksheet 1 from the MARC BMP Manual was completed for this development and is included in Appendix E. The site requires a Level of Service of 7. BMP stormwater requirements for this site will be met by installing: native vegetation, value rating = 9.25, extended wet detention basin for the 90% Mean Storm, value rating = 5.0, with 1 acre routed to a native swale. Infiltration trenches and native swales will also be installed.

Native vegetation will be preserved and established surrounding the proposed buildings and associated pavements and will have a total planting area of 5.02 acres, with 1 acre routed to an infiltration trench. The extended wet detention basin will be for 7.60 acres in area and will lead to a native swale. There will also be infiltration trenches and a native swale. For native areas routed to a detention trench, the proposed area shall be a minimum of 2484 square feet, and the depth of the trench shall be 7.50 feet. The infiltration trench will be constructed with this project, therefore a percolation test is not required.

The 90% mean storm 40 hour detention design for the 13.62 acres of site disturbance is located in Appendix E. Proposed detention will be provided by a perforated riser for the detention basin.

We separated the storm water volume mitigation and the storm water quality mitigation due to the fact that we are utilizing an existing pond and converting it to an extended wet detention basin for the 2-year, 5-year, 10-year, 25-year, 50-year, and 100-year events. The existing pond is not large enough to provide the 90% mean storm 40 hour detention. Therefore, we moved the 90% mean storm detention basin adjacent to the stream.

CONCLUSIONS AND RECOMMENDATIONS

The proposed project will cause an increase of runoff after the improvements are made to the site. We are proposing to reduce the 5-year, 10-year, 25-year, 50-year, and 100-year storm events runoff rates to below existing conditions with two extended wet detention basins. The 2-year event does increase by 0.30 cfs, (less than a 0.28% increase.) The proposed BMP's meet the Level of Service of 7.

Based on the information provided herein we request your approval of this storm drainage study for Bella Vista at the National.

CUT	FILL	NET
134006	153120	19114 (F) Grid
134731	154403	19672 (F) Composite
134785	155070	20284 (F) End area:
	AVERAGE	19,690 (F)

APARTMENT PARKING COUNT

TOTAL UNITS: 300

BUILDING	UNDER BUILDING	OUTSIDE	OUTSIDE GARAGE	TOTAL
A	80	96	19	195
B	80	79	0	159
C	80	95	27	202
TOTAL				556
				1.85 STALLS / UNIT

OUTBUILDING DATA

2-STORY BUILDING 7,500SF
 PARKING 71 - 9.46 STALL / 1000sf

STEVEN A. WARGER
 NO. LICENSE NO. E-20997

DATE PREPARED:
 3-27-15

ROUTE	STATE
DISTRICT	SHEET NO.

COUNTY

JOB NO.

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

REVISIONS	DATE

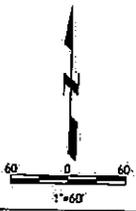
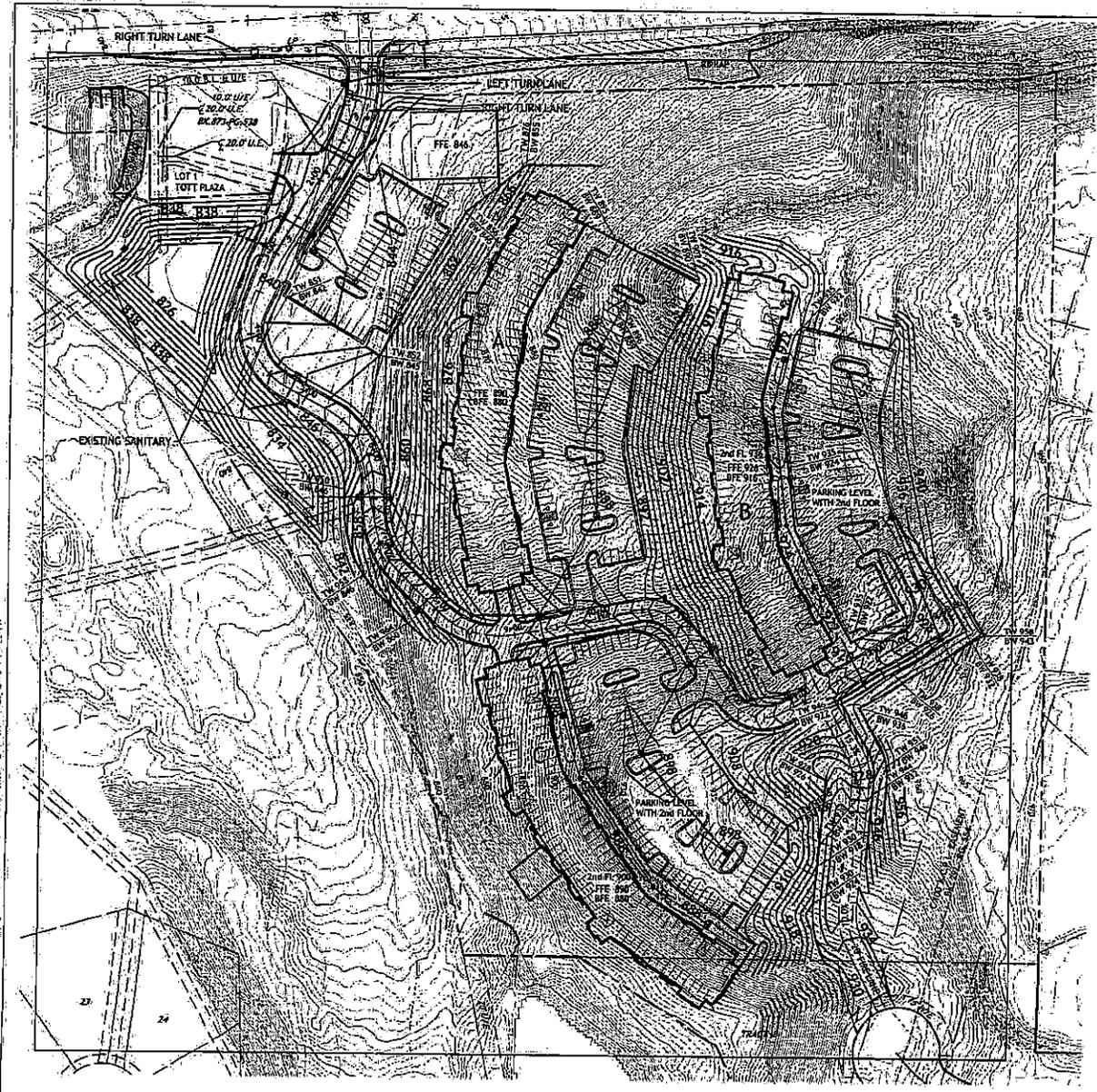


WARGER ASSOCIATES
 Consulting Engineers
 1677 Swift
 North Kansas City, Missouri, 64116
 913.268.9132
 Steve@wargerassociates.com

BELLA VISTA AT
 THE NATIONAL

CONCEPT 0

SHEET 2 OF 3



15.00' 10.00' 5.00' 0.00' 5.00' 10.00' 15.00' 20.00' 25.00' 30.00' 35.00' 40.00' 45.00' 50.00' 55.00' 60.00' 65.00' 70.00' 75.00' 80.00' 85.00' 90.00' 95.00' 100.00'



CITY OF PARKVILLE • 8880 Clark Avenue • Parkville, MO 64162 • (816) 741-7676 • FAX (816) 741-0018

May 8, 2015

Mr. Steve Warger, P.E.
Warger Associates LLC
1617 Swift
N. Kansas City, MO 64116

RE: Bella Vista at the National – Stormwater Study

Dear Mr. Warger:

The City of Parkville has reviewed the Preliminary Stormwater Study submitted for the Bella Vista Apartments located in the southeast corner of Hwy 45 and Lake Crest Lane, dated May 7, 2015.

The Preliminary Stormwater Study is approved for consideration of the Preliminary Development Plan application.

Additional design information related to the Stormwater Detention and Stormwater Treatment Facilities will be necessary in the Final Stormwater Study, which will be submitted with the construction plans.

Please feel free to contact me if you have any questions.

Sincerely,

A handwritten signature in cursive script that reads "Alysen M. Abel".

Alysen M. Abel, P.E.
Public Works Director

cc: Sean Ackerson, Assistance City Administrator / Community Development Director

Bella Vista Apartments

TRAFFIC IMPACT STUDY

May 7, 2015

Prepared For:
Warger Associates, LLC
1617 Swift
North Kansas City, MO 64116

Prepared By:
Priority Engineers, Inc.
PO Box 563
Garden City, MO 64747





Priority
ENGINEERS, INC.

May 7, 2015

Mr. Steve Warger
Warger Associates, LLC
1817 Swift
North Kansas City, Missouri 64116

Re: Bella Vista Apartments - Parkville, MO

Dear Mr. Warger:

In response to your request, Priority Engineers, Inc. has completed a traffic impact study for the above referenced project. The purpose of the analysis is to determine the potential traffic impacts associated with this development on the intersections and streets surrounding this site, primarily during the AM and PM peak hours. The following report documents our analysis and recommendations.

We appreciate the opportunity to work with you on this project. Please contact us with any questions or if you require additional information.

Sincerely,

PRIORITY ENGINEERS, INC.

Kristin L. Skinner, P.E., PTOE
President

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1) INTRODUCTION

The purpose of this study is to examine the potential traffic impacts associated with the proposed Bella Vista Apartments and a VFW Hall located on the southeast corner of Missouri Highway 45 and Lakecrest Lane in Parkville, Missouri. Access to the apartments will be through an extension of Lakecrest Lane to the south.

The study area is shown in Figure 1. The site layout is shown in Figure 2.

2) EXISTING CONDITIONS

The existing site consists of a wooded property on the south side of Missouri Highway 45, to the east of Lakecrest Lane. Lakecrest Lane currently provides access to a dental office and ends approximately 250' south of the dental office drive.

Missouri Highway 45 is a four lane arterial roadway with a raised median. The posted speed limit adjacent to the property is 45 miles per hour and transitions to 35 miles per hour to the east at Graden Road.

The site is bordered to the west by a golf course and a dental office. To the southwest are single family homes which transition to multi-family units to the southeast. To the east of the property is a commercial/small industrial area which has primary access to Missouri Highway 45 through the Bell Road intersection. On the north of Missouri Highway 45, there is a gas station and commercial uses with a full access driveway opposite Lakecrest Lane.

Peak Hour turning movement traffic counts for the Missouri Highway 45 intersections with Blair Road, Lakecrest Lane, Graden Road, and Bell Road. These counts were conducted between March 3-5, 2015 between the hours of 7:00 and 9:00 AM and from 4:00 to 6:00 PM. The peak hours were determined to be 7:00 to 8:00 AM and from 5:00 to 6:00 PM. The complete traffic counts are shown in Appendix II. The peak hour traffic volumes and existing lane configurations are shown in Figures 3-7.

3) PROPOSED DEVELOPMENT

The Bella Vista Apartments will consist of 300 units in three separate buildings. The VFW will be 3,500 square feet and will be located on the east side of Lake Crest. The proposed site plan is shown in Figure 2. As part of this development, Lakecrest Lane will be extended to the southeast to provide access to the proposed apartment buildings. An additional 12' fire lane will be provided off of Lime Stone Road. This access will be gated and will not be an available access for apartment residents.

Lakecrest Lane will be widened to include a short left turn lane and a through/right lane. Additionally, an eastbound right turn lane will be constructed on Missouri Highway 45.

4) TRIP GENERATION

The vehicle trips generated by the proposed development were estimated using the Institute of Transportation Engineers' Trip Generation, 9th Edition. Land Use 220, Apartment, was used for the apartments. Land Use 591, Lodge/Fraternal Organization, was considered for the VFW Hall. However, this use has only one site that was surveyed in 1977. Instead, Land Use 925,

Drinking Place, was used. The estimated AM and PM peak hour traffic volumes associated with these uses are shown in Table 1.

Table 1: Trip Generation							
Land Use	Intensity	AM Peak			PM Peak		
		Total	In	Out	Total	In	Out
Apartment	300 units	153	31	122	186	121	65
Drinking Place	3,500 SF				40	26	14
Total		153	31	122	226	147	79

5) TRIP DISTRIBUTION

Trips generated by the Bella Vista Apartments were distributed based on existing traffic flows and a general analysis of the surrounding area. The trips were distributed onto the existing street system approximately as follows:

- 60 percent to/from the east on Missouri Highway 45
- 25 percent to/from the west on Missouri Highway 45
- 7 percent to/from the south on Bell Road via Missouri Highway 45 to the east
- 6 percent to/from the north on Lakecrest Lane (to the commercial properties)
- 1 percent to/from the north on Graden Road via Missouri Highway 45 to the east
- 1 percent to/from the north on Blair Road via Missouri Highway 45 to the west

The proposed development trips are shown in Figures 7-8.

6) SIGHT DISTANCE

Intersection sight distance was verified for the Lakecrest Lane intersection with Missouri Highway 45. Intersection sight distance represents the distance and time required for the driver to make the decision to turn and to complete the turn without slowing oncoming traffic. The sight distance to the east and to the west exceeds 1000'. The required intersection sight distances are illustrated in Table 2 below.

Table 2: Sight Distance Values			
	Measured Distances	AASHTO Distances (45 mph)	AASHTO Distances (50 mph)
Lakecrest Lane			
To the East	>1000'	530'	590'
To the West	>1000'	430'	480'

7) SIGNAL WARRANTS

The Manual of Uniform Traffic Control Devised (MUTCD) peak hour signal warrants were consulted for the intersection of Lakecrest Lane and Missouri Highway 45. Due to the 45 mile per hour speed limit on Missouri Highway 45, the reduced (70%) peak hour factor was consulted. With the reduced factor, a traffic signal is warranted during the AM and PM Peak Hour.

Traffic volumes on Lakecrest Lane are not expected to increase after this development is constructed. There are several variables that may make a signal at this location unnecessary. Under existing conditions, the speed limit on Missouri Highway 45 lowers to 35 miles per hour to the east of this site prior to Graden Road. With the construction of this project, and the existing commercial uses on the north side of Missouri Highway 45, the 35 mile per hour zone could be extended further to the west to include this development. Lowering the speed limit to 35 miles per hour would make a signal at this location unnecessary.

8) AUXILIARY TURN LANES

Section 940.9 of the Missouri Department of Transportation Engineering Policy Guide was consulted to determine that an eastbound right turn lane would be required at the Lakecrest Lane and Missouri Highway 45 intersection. This turn lane should be constructed with 120' deceleration length and a 100' taper.

9) LEVEL OF SERVICE AND VOLUME/CAPACITY ANALYSES

Capacity analysis was used to quantify the impacts of the increased traffic on the intersections studied. The methodology outlined in the Highway Capacity Manual, 2000 Edition, was used as a basis to perform the analysis for this study. Capacity analysis defines the quality of traffic operation for an intersection using a grading system called Level of Service (LOS). The LOS is defined in terms of average vehicle delay. Levels of service A through F have been established with A representing the best and F the worst.

Table 4: Level of Service Definitions		
Level of Service	Unsignalized Intersection	Signalized Intersection
A	< 10 Seconds	< 10 Seconds
B	< 15 Seconds	< 20 Seconds
C	< 25 Seconds	< 35 Seconds
D	< 35 Seconds	< 55 Seconds
E	< 50 Seconds	< 80 Seconds
F	≥ 50 Seconds	≥ 80 Seconds

The study intersections were evaluated using Synchro, an analysis package based in part on Highway Capacity Manual methods. The analysis reports are included in Appendix II.

Existing Conditions

The levels of service, lane configuration, and queue lengths for existing conditions are shown in Figures 5 and 6 in Appendix I. The overall level of service at the signalized intersections is a B

in both the AM and PM Peak Hour. Southbound Graden Road and southbound Lakecrest Lane experience levels of service D and E during the peak hours. Low levels of service at stop controlled intersections during peak hours are not uncommon, and the queue lengths are less than two vehicles.

Proposed Conditions

This scenario represents the existing traffic combined with the full build out of the Bella Vista Apartments and the VFW Hall. The levels of service and queue lengths at the surrounding intersections are only slightly impacted by the additional development traffic on Missouri Highway 45. At the intersection of Lakecrest Lane with Missouri Highway 45, the southbound movement lowers to a level of service C in the AM peak hour and an F in the PM Peak hour. The northbound left, exiting the Bella Vista apartments, is a level of service C in the AM Peak Hour and a level of service F in the PM Peak hour. The 95th percentile queue, for this movement will be approximately one vehicle in both the AM and PM. Although this intersection could be signalized, as mentioned in Section 7 of this report, low levels of service during peak hours at stop controlled intersections are not uncommon, and the queue length will not be excessive.

Levels of service, lane configuration and design queue lengths for the proposed conditions are shown in Figures 9 and 10.

10) FUTURE CONDITIONS

A future scenario was created in order to estimate traffic volumes through study intersections in the year 2035. A 2% growth factor was applied to background traffic volumes to generate this scenario which is illustrated in Figures 11-14 in Appendix I. Additionally, trips were estimated for the undeveloped site on the north side of the Bell Road and Missouri Highway 45. A site plan is not available for this site, but trips equivalent to a 24,000 square foot supermarket and a 6,400 square foot high-turnover sit down restaurant were assumed.

As traffic volumes on Missouri Highway 45 increase, left turns at stop controlled entrances become increasingly difficult with increased delays. The level of service at the Lakecrest Lane intersection drops to an E in the AM Peak Hour and drops to a F in the PM Peak Hour with a design queue of approximately 4 vehicles.

11) RECOMMENDATIONS & CONCLUSIONS

This study documents the impact of the proposed Bella Vista Apartments and the VFW Hall on the adjacent intersections during the AM and PM peak hours. As a result of this development Lakecrest Lane will be extended to the south to provide access to the proposed site. The intersection of Lakecrest Lane and Missouri Highway 45 should include the following improvements:

- Northbound left/through turn lane
- Eastbound right turn lane – 120' minimum length with 100' taper

Additionally, one of the following three recommendations should be considered for the Lakecrest Lane and Missouri Highway 45 intersection:

- The transition from a 45 mph to a 35 mph speed limit along Missouri Highway 45 should be moved to the west of this intersection (currently this transition takes place to the west of Graden Road)
- Following the construction and occupation of two of the three apartment buildings, the need for a signal at the intersection of Lakecrest Lane and Missouri Highway 45 should be reassessed.



MEMORANDUM

Missouri Department of Transportation
Traffic
KC

TO: Priority Engineers

CC: Nathan Juliana
Jim Burgess

FROM: Adam Wood
Traffic Studies Specialist

DATE: 5-19-15

SUBJECT: Traffic Impact Study
Bella Vista Apartments
Platte County

After reviewing the traffic impact study for the Bella Vista Apartments which is dated May 7, 2015, the Missouri Department of Transportation has the following comments:

- Signal Warrant #3 shall only be used in unusual cases, such as office complexes, manufacturing plants, industrial complexes, or high occupancy vehicle facilities that attract or discharge large numbers of vehicles over a short time. This warrant does not need to be used for the Bella Vista Apartments.
- The SB MO 45 right turn lane can be built as a taper entrance, similar to the right turn entrance at SB MO 45 and NW Hogan Drive. Final plans and dimensions will still need to be approved.



AT&T Network Operations
500 E 8th, Room 1152
Kansas City, MO 64106

T: 816-275-3850
F: 816-283-8980
JB0311@att.com

May 26th, 2015

Warger Associates
Attn: Steve Warger

**RE: Bella Vista at the National
Parkville, MO**

Dear Mr. Warger:

In response to your request concerning the availability of **Telephone Service** to the above referenced apartment complex, please be advised that we will be able to provide telephone service to you under our approved terms and conditions of service.

Should you need further information, feel free to contact us at any time.

Sincerely,

Jill Schroeder
Manager OSP Planning & Engineering Design
AT&T
500 E 8th, Room 1152
Kansas City, MO 64106
816 275-3850 phone
816 283-8980 fax

-  Bella Vista-Sheet 01 Prelim plan (5).pdf
613K
-  Bella Vista-Sheet 01 Prelim Plat (1) 2015.5.15.pdf
572K
-  ATT00002.htm
1K
-  Bella Vista CONCEPT 4-3-15-Sheet 02 grading.PDF
2021K
-  ATT00003.htm
1K

steve warger <stevewarger@gmail.com>
 To: Sean Ackerson <sackerson@parkvillemo.com>
 Cc: Adam THOLEN <adamtholen@yahoo.com>, David Pence <david@pricearchitecture.com>

Tue, May 26, 2015 at 12:30 PM

See below
(Quoted text hidden)

-  Will serve letter-Bella Vista.doc
86K

Rudler Brad <Bradley.Rudler@kcpl.com>
 To: steve warger <stevewarger@gmail.com>, "Richards, Donnie" <dennie.richards@thelacledgroup.com>, "Passantino, Beverly" <Beverly.Passantino@sug.com>

Wed, May 27, 2015 at 8:21 AM

We won't have any trouble serving this area. Fill out the app I am attaching when you are ready for us to start designing and send us the autocad file and I can get started on it.

thanks

*Brad Rudler
 General Design Tech
 Kansas City Power and Light
 Northland Service Center
 8325 N Platte Purchase DR
 Kansas City, MO 64118-1057
 phone - 816-420-4732
 fax - 816-420-4799
 cell - 816-223-7790
 bradley.rudler@kcpl.com*

KCPL

From: steve warger [mailto:stevewarger@gmail.com]
Sent: Tuesday, May 26, 2015 9:19 AM
To: MANION, MARK A; SCHROEDER, JILL A (ATTSWBT); Rudler Brad; Feltes Melissa; Richards, Donnie; Passantino, Beverly
Subject: Bella Vista at the National

This is an EXTERNAL EMAIL. Stop and think before clicking a link or opening attachments.

To all:
(Quoted text hidden)

-  Multi-Fam Svc App.doc
26K

steve warger <stevewarger@gmail.com>
 To: David Pence <david@pricearchitecture.com>
 Cc: Adam THOLEN <adamtholen@yahoo.com>

Wed, May 27, 2015 at 8:23 AM

(Quoted text hidden)

-  Multi-Fam Svc App.doc
26K

steve warger <stevewarger@gmail.com>
 To: Sean Ackerson <sackerson@parkvillemo.com>

Wed, May 27, 2015 at 8:23 AM

KCPL can serve

Tue, May 26, 2015 at 11:43 AM

Passantino, Beverly <Beverly.Passantino@thelacledegroupp.com>
To: "STEVEWARGER@GMAIL.COM" <STEVEWARGER@gmail.com>

Steve,

This will confirm that there are adequate natural gas services and facilities to serve the Bella Vista at the National project.

Sincerely,

Beverly Passantino
Business Development Representative
Missouri Gas Energy/Laclede Gas Company
7500 E 35th Terrace
Kansas City, MO 64129
Office Phone:(816) 472-3434
E-mail: Beverly.Passantino@thelacledegroupp.com
For rebates: www.betterheatingnow.com

MGE

From: Richards, Donnie
Sent: Tuesday, May 26, 2015 9:48 AM
To: Passantino, Beverly
Subject: Fwd: Bella Vista at the National

Sent from my iPhone

Begin forwarded message:

From: "steve warger" <stevewarger@gmail.com>
To: "MANION, MARK A" <MM256T@att.com>, "SCHROEDER, JILL A (ATTSWB)" <jb0311@att.com>, "Rudler Brad" <Bradley.Rudler@kcpl.com>, "Melissa Feltes" <melissa.feltes@kcpl.com>, "Richards, Donnie" <Donnie.Richards@thelacledegroupp.com>, "Passantino, Beverly" <Beverly.Passantino@sug.com>
Subject: Bella Vista at the National

To all:

[Quoted text hidden]
816-769-6132<tel:816-769-6132>

6 attachments

-  Bella Vista-Sheet 01 Prelim plan (5).pdf
613K
-  ATT00001.htm
1K
-  Bella Vista-Sheet 01 Prelim Plat (1) 2015.5.15.pdf
572K
-  ATT00002.htm
1K
-  Bella Vista CONCEPT 4-3-15-Sheet 02 grading.PDF
2021K
-  ATT00003.htm
1K

steve warger <stevewarger@gmail.com>
To: Sean Ackerson <sackerson@parkvillemo.com>
Cc: Adam THOLEN <adamtholen@yahoo.com>, David Pence <david@jpricearchitecture.com>

Tue, May 26, 2015 at 11:44 AM

See below. MGE is good
[Quoted text hidden]

6 attachments

-  ATT00001.htm
1K

BILL NO. 1827

ORDINANCE NO. 1816

AN ORDINANCE APPROVING AMENDMENTS TO THE COMMUNITY UNIT PLAN FOR THE NATIONAL, INCLUDING REZONING OF CERTAIN PARCELS FROM SINGLE-FAMILY TO MULTI-FAMILY, AND CERTAIN OTHER PARCELS FROM MULTI-FAMILY TO SINGLE-FAMILY.

BE IT ORDAINED BY THE BOARD OF ALDERMEN OF THE CITY OF PARKVILLE, MISSOURI, AS FOLLOWS:

Section 1. The amendments to the Community Unit Plan as shown on the plan attached hereto and incorporated herein as Exhibit A, and in the letter from The National dated July 13, 1999, and addressed to the Parkville Planning Commission, attached hereto and incorporated herein as Exhibit B, are hereby approved with the following conditions:

- A. A street right of way shall be established between Tract CC and Bell Road.
- B. It is agreed that, in lieu of park land, The National will pay to the City a sum of \$ 125,000 to go into a fund for a trail along South Crooked Road.
- C. The street alignment at the Crooked Road entrance is subject to further review before approval of that feature of the plan.
- D. Stormwater drainage from the project is subject to continuing review by City engineers for compliance with APWA standards to safeguard against contributing to an increase in storm drainage problems downstream.

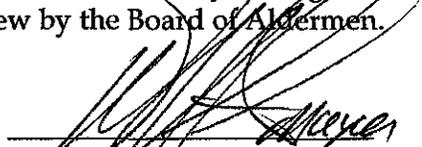
Section 2. Zoning changes from the Community Unit Plan as originally submitted include:

- A. Tract R, formerly designated for multi-family, is now zoned Single-Family with a density of R-2, which obtains throughout the CUP.
- B. Tract CC, not heretofore specifically zoned except as part of the overall CUP R-2, is now zoned Multi-Family R-5, planned multi-family zoning which requires initial approval and continuing review by the Board of Aldermen.

PASSED this 20th day of July, 1999.

ATTESTED:


City Clerk Barbara J. Lance


Mayor William M. Quitmeier

APPROVED this 20th day of July, 1999.


Mayor William M. Quitmeier

ORD. NO. 1816



To: Parkville Planning Commission

From: The National

Date: July 13, 1999

Re: Revisions to The National's C.U.P.

The National is submitting its revised CUP plan to you for your consideration. The changes that have been made to the plan truly make it a better community. The following narrative summarizes the major changes that were made. The attached drawing now shows the entire CUP on one sheet which makes it much easier to understand. In addition, it is much easier for the City to keep this information on file.

This development has extremely low density. The total dwelling units per acre is just over one (1), which is almost unheard of in today's development world. When the multi family units and acreage are taken out (the multi family density is based on the maximum possible units, the final site plan will undoubtedly come in with less units), the dwelling units per acre drops to less than one (1) at 0.82. The design of the development has gone to great lengths to save as many of the trees and other natural features as possible. In fact the part of the development that is south of Highway 45 has been reconfigured in order to keep the roads much closer to existing grade in order to save thousands of trees. The number of single family lots has been reduced to 292 from 378 on the same amount of ground. We have incorporated several changes recommended by the City's review engineer at Shafer, Kline & Warren into our plans. The changes will be beneficial to the City in the future.

Golf course and open space

Originally, north of Highway 45 there was to be the 18 hole Tom Watson signature, private golf course, and 9 holes of the semi-private golf course. The remaining 9 holes of the semi-private course would have been located south of Highway 45 and would have been connected by a tunnel under the highway. The thought was that the 9 holes north of the highway would use some of the original holes from the old Windbrook and River Park courses. After looking at the situation, the designers decided that it would be better to have 7 semi-private holes north of Highway 45 and 11 south. All of the holes will be a new design. Therefore, we will have a new 36-hole complex at The National that will compete with any other golf community in the Midwest. The elevation changes of the

property, as well as the natural water features that exist will make these holes truly spectacular. The north and south properties will still be connected by a pedestrian/cart tunnel.

There have been several areas of open space added to the plan and are labeled as such. These areas are in strategic locations to serve as buffers between existing or proposed neighborhoods or public roads. Several of these changes were the result of good input from the Planning Commission and Board of Alderman at previous meetings. Those include adding a buffer and open space tract adjacent to Crooked Road, both north and south of Highway 45. There has also been open space added to landscape islands on National Drive south of Highway 45. These serve two functions, they give motorists or pedestrians views of nice landscaped areas as they travel through the neighborhoods. More importantly though, they eliminate driveways from the main road and moves them to the access roads that surround the islands.

There are over 425 acres of golf course and open space associated with the development. With this much green area in the neighborhoods, it was decided that it would be repetitive to develop small neighborhood parks that would really only be used by the residents of that particular neighborhood. Therefore, we felt that in order to capitalize on the existing assets that Parkville already has in English Landing Park, the White Alloe Conservation area, and the Downtown area, we would like to assist in getting our residents to those areas. It was felt that by using the cash in lieu of parkland dedication, we could donate money to Parkville to be used to develop a trail system down Crooked Road. By using our sidewalk system and the tunnel under Highway 45 we can ensure that residents from the very northern area of Parkville can travel on a trail system to English Landing Park. Trails are a hot topic everywhere right now. By using our cash in lieu of parkland to develop the trail down Crooked Road, combined with our sidewalks and the existing trails in the conservation area and English Landing, Parkville will have an elaborate, functional trail system that any city would envy.

Multi-family residential

You will recall that the area shown as Area R on the new CUP was originally designated as multi-family. After the golf layout changed, this left an area that was ideal for single family cottage homes similar to what will be built in Areas D & E. This product will be a maintenance free neighborhood with a privately maintained street. Every lot in the neighborhood is on-golf, with some also being on water. To our knowledge there is not a similar community anywhere in the area. For these reasons, we would like to remove the multi -family zoning from this area and change it to R-2 CUP, which is the same zoning as the remainder of our single-family property. The reduction in density should allow this area to blend much better with the surrounding neighborhoods.

Area CC on the CUP is now requested to have a R-5 CUP zoning classification. In reading the Parkville Zoning Code it is almost like the zoning district was written for this particular piece of property:

"This district provides primarily for the highest and best use of land lying between land zoned residential on one (1) side and land zoned business or industrial on another, striving for the retention of the highest value for all properties."

The R-5 zoning will allow the Board of Alderman to have final say on several design issues once a site plan is developed and presented for this area. The types of units planned for this area are very upscale with stone, private garages, neighborhood amenities and other things that will make this very different from a typical upscale multi-family development.

Single-family residential

The single-family layout has changed somewhat in the revised plan from what was previously approved. Starting in the northwest corner of our property, areas G and H have been revised to reflect the comments by the Planning Commission and Board of Alderman. There has been a landscape buffer shown along Crooked Road. There was always a buffer planned, but it was not depicted on the previous plan. In addition, the intersection is proposed to be realigned to allow for greater separation between this intersection and the Crooked Road intersection. The road also has some curve added to it to break up the view and work with the natural topography.

The only other major change on the property north of Highway 45 has been previously mentioned. It is the removal of multi-family units and the addition of single-family homes in Area R. It is felt that these lots are a perfect fit in the area surrounded by golf holes on all sides. As discussed this area will be maintenance provided community accessed by a privately maintained road.

On the south side of Highway 45 the layout has changed for what we feel is much better. The roads are much closer to natural grades which will mean that many trees will be able to be salvaged. Trees make any development much better. Our philosophy all along has been to keep as many trees as possible. Anything we can do to further that goal is highly desirable.

You will notice that there have been several landscaped islands added to the main road, National Drive. These serve two functions. They provide a visually appealing look to the pedestrian or motorist travelling through the community giving them glimpses of green areas instead of a row of houses. In addition, they also keep additional driveways from accessing onto National Drive. This will provide for a much safer and more efficient flow of traffic.

There has been additional landscaping added to the area around the existing and proposed ponds in the southwest part of the property to provide a park like setting for relaxation and reflection. This area will provide a great buffer between the lots that back up near each other in Area Z that have been reconfigured with this area as a focal point.

The single-family lots in Area AA have been revised to provide for much more of a neighborhood than before. The open space at the entrance, as well as the village green and open space added to the area will allow this area to be a part of the overall community but also have its own distinct character and feeling. The village green will take residents back to a time when these types of areas were used as gathering places for block parties or just spending time with the family.

Conclusion

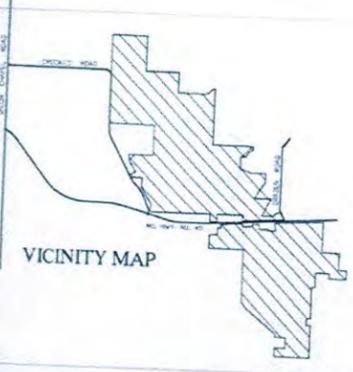
This highlights the major changes that have been made to our new CUP. The changes that were made should make The National a much more cohesive development, but we feel that they also complement the existing assets and charm of Parkville. Hopefully, the attached CUP plan will adequately show the layout of The National, but of course we will be happy to address any questions you might have at the upcoming hearing.



DEVELOPMENT SUMMARY

TRACT	TRACT	LAND USE	ZONING	DWELLING UNITS	LOT WIDTH
A	11.45 Ac	Single Family Lots	R-2	53	120-150
B	11.12 Ac	Single Family Lots	R-2	25	110-130
C	11.25 Ac	Single Family Lots	R-2	24	10-60
D	6.05 Ac	Single Family Lots	R-2	4	50
E	26.33 Ac	Single Family Lots	R-2	11	90
F	25.45 Ac	Single Family Lots	R-2	30	110-130
G	5.02 Ac	Single Family Lots	R-2	24	90-120
H	3.00 Ac	Single Family Lots	R-2	14	70
I	8.19 Ac	Single Family Lots	R-2	14	60
J	11.77 Ac	Club House/Tennis	R-2	25	65-70
K	2.80 Ac	Single Family Lots	R-2	8	100
L	4.07 Ac	Lodge Site	R-4	-	-
M	5.50 Ac	Farmway Gutters	R-2	-	-
N	1.03 Ac	Turf Care Center	R-2	-	-
O	1.80 Ac	Commercial Site	B-4	-	-
P	0.28 Ac	Marina Site	R-2	-	-
Q	1.53 Ac	Single Family Lots	R-2	57	60
R	9.32 Ac	River Park Clubhouse	R-2	-	-
S	2.81 Ac	Single Family Lots	R-2	4	40
T	4.31 Ac	Single Family Lots	R-2	27	90-100
U	11.28 Ac	Single Family Lots	R-2	19	90-95
V	12.93 Ac	Single Family Lots	R-2	26	95-99
W	8.62 Ac	Single Family Lots	R-2	25	90-99
X	14.21 Ac	Single Family Lots	R-2	24	60
Y	25.30 Ac	Single Family Lots	R-2	40	90-95
Z	26.30 Ac	Single Family Lots	R-2	25	90-100
AA	2.57 Ac	Single Family Lots	R-2	6	90-95
BB	4.99 Ac	Multi Family	R-5	368	-
CC	3.20 Ac	Commercial Site	B-4	-	-
DD	1.40 Ac	Commercial Site	B-4	-	-
EE	4.26 Ac	Commercial Site	B-4	-	-
FF	8.50 Ac	Commercial Site	B-4	-	-
Subtotal	358.63 Ac				
GOLF COURSE	42.97 Ac				
RIGHT OF WAY	10.00 Ac				
TOTAL	411.60 Ac				
DENSITY				810.00 TOTAL	86 MULTI-FAMILY UNITS 41 UNITS/FAMILY UNITS 120 DCA
TOTALS INCLUDE 10 EXCEPTED LOTS					

- GENERAL NOTES:**
- THIS PLAN IS FOR PRELIMINARY USE ONLY AND DOES NOT REPRESENT A BOUNDARY SURVEY.
 - SITE PLAN IS CONCEPTUAL IN NATURE AND IS SUBJECT TO CHANGE.
 - BOUNDARY SURVEY INFORMATION PROVIDED BY R.L. BIRDFOOT ASSOCIATES INC. (866) 748-6152.
 - ALL DIMENSIONS SHOWN HEREON ARE APPROXIMATE AS DETERMINED BY SCALE ON DED AND ARE SUBJECT TO EXECUTION OF A BOUNDARY SURVEY AND FINAL PLAT COMPUTATIONS.
 - IF PORTION OF THE PROPERTY SHOWN HEREON DOES LIE WITHIN A FLOOD PRONE AREA ACCORDING TO THE FEMA FLOOD MAPS FOR THE SUBJECT AREA.
 - ALL UTILITIES ARE AVAILABLE AND WILL BE EXTENDED INTO THE PROPOSED DEVELOPMENT.
 - UTILITY EASEMENT WILL BE GRANTED BY THE FINAL PLAT ALONG ALL UTILITY LINES AND ALONG ALL STREET RIGHT-OF-WAY LINES.
 - PROPOSED STREETS SHALL BE 6' AND 2' ASPHALT PAVEMENT WITH CONCRETE CURB AND GUTTERS HAVING THE FOLLOWING DATA:
STREET CLASS: RESIDENTIAL LOCAL STREET, PARKWAY, BOULEVARD STREET
RIGHT OF WAY: 50 FT, 75 FT, 100 FT, 125 FT
SIDEWALK: 5 FT, 7 FT, 10 FT
MAX GRADE: 5%, 6%, 7%
 - SIDEWALKS SHALL BE CONSTRUCTED ON ONE OR BOTH SIDES OF ALL STREETS BY THE HOME BUILDER. SIDEWALKS TO BE UTILIZED AS TRAIL SYSTEM THROUGHOUT DEVELOPMENT.

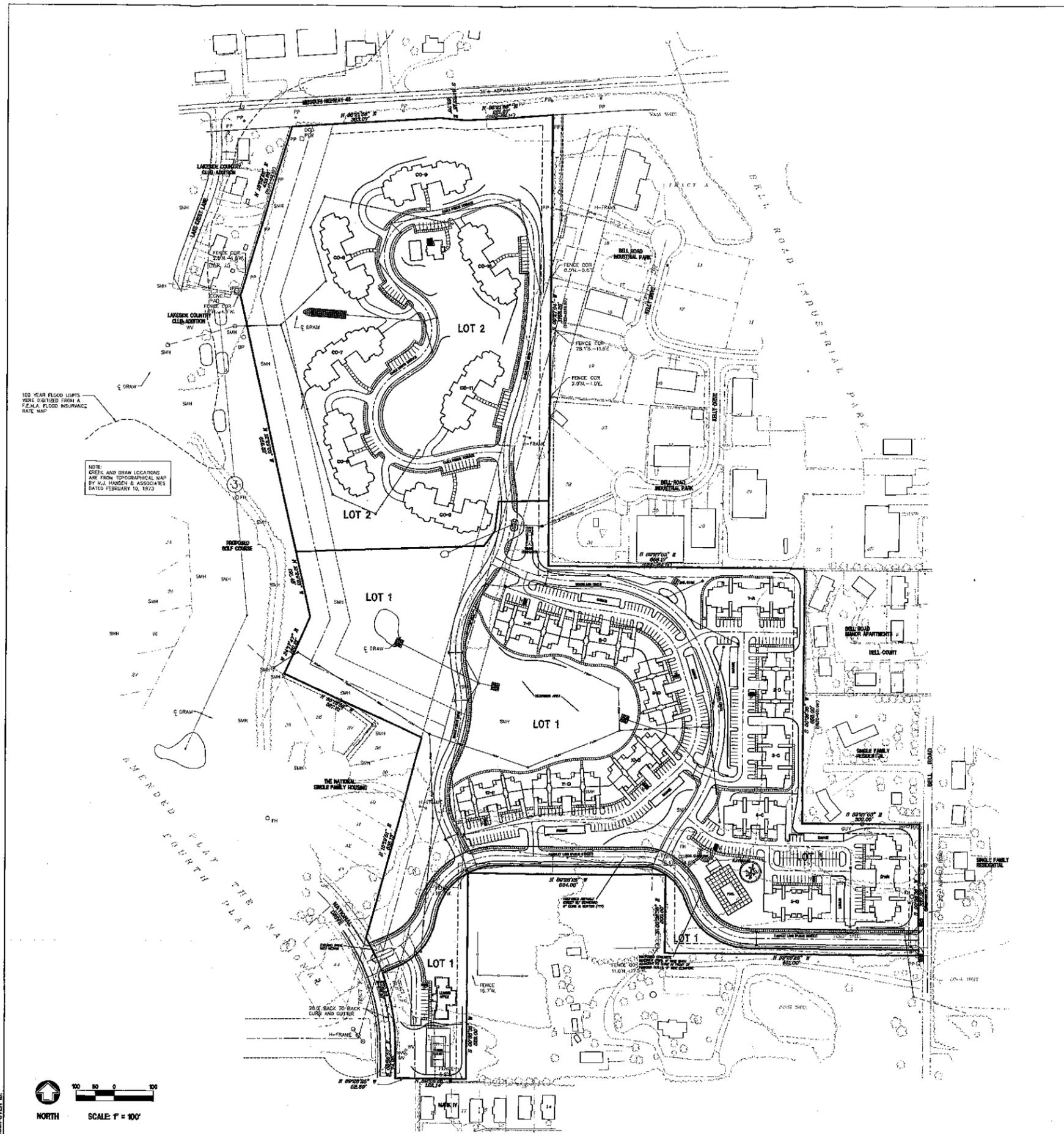


PROPOSED COMMUNITY UNIT PLAN
THE NATIONAL GOLF CLUB
 OF KANSAS CITY
 CITY OF PARKVILLE, PLATTE COUNTY, MISSOURI
 DEVELOPED BY
STERN BROTHERS REAL ESTATE FINANCE
 GOLF COURSE DESIGN
TOM WATSON DESIGN



Handwritten Signature

DATE: 6/17/08
 PROJECT NO: 1800
 REVISION:
 REV. 10-1-06 PER CLIENT
 REV. 10-9-06 ACCORD ZONING
 REV. 10-21-06 REVISED PER CLIENT
 REV. 3-9-09 REVISED PER CLIENT
 SHEET NO: 1
 FILE NAME: 620802000000_001.dwg
 © 2008 LDI Design Inc.
LDI Design Inc
 Landscape Architecture Civil Engineering
 Surveying Planning Geographic Information Services
 REV. 4-7-09 REVISED PER CLIENT
 REV. 7-16-09 ACCORD ZONING
 REV. 2-9-10 REVISED PER CLIENT



GENERAL NOTES

- Property Owner of Record & Developer: Thomas Fritzell, P.O. Box 721, Lawrence, KS 66044, (785) 841-6346. Landplanner/Engineer: Landplan Engineering, P.A., 1600 Genesee, Suite 400, Kansas City, MO 64102.
- Topographic information obtained from field survey by R.L. Buford & Associates. Contour interval is (2) feet.
- Site utilities as shown on plan shall connect to public utilities.
- All utility lines to be underground.
- Access for the site shall be limited to the points shown on the plan.
- Trash for apartments shall be collected by trash dumpsters as shown on the site plan.
- This development plan provides a controlled development of defined density located within the city limits.
- Streets and off street parking areas to be constructed per City of Parkville standard details.
- Proposed concrete walks to be 4" Portland cement concrete on compacted subgrade.
- Lawn areas shall be K-31 Fescue.
- Site plan has been designed to comply with minimum provisions of the City of Parkville Site Plan Guide.
- All parking stalls are 9' W x 19' L.
- The proposed street and storm sewer improvements shall conform to the current standards and specifications of the Department of Public Works, Engineering Division, City of Parkville, Missouri and the Kansas City Metropolitan Chapter American Public Works Association (APWA).
- The proposed sanitary sewer improvements shall conform to the current standards and specifications of the Department of Public Works, Engineering Division, City of Parkville, Missouri and the Kansas City Metropolitan Chapter American Public Works Association (APWA).
- The proposed waterline is approximate. Final location will be determined at time of Construction Documentation.
- Wall packs are proposed for lighting of apartment parking lots. The main drives will be illuminated using 250 watt H.P.S. pole mounted lights spaced at 150 ft. intervals. Back shields will be used to minimize off-site light run-off.

SITE SUMMARY

APARTMENTS (LOT 2)				
	UNITS EACH	TOTAL UNITS	PARKING REQUIRED	PARKING SHOWN
A BUILDINGS	24 UNITS	48 UNITS	96 SPACES	16 ATTACHED GARAGES 16 DRIVEWAY
B BUILDINGS	24 UNITS	24 UNITS	48 SPACES	4 ATTACHED GARAGES 4 DRIVEWAY
C BUILDINGS	22 UNITS	44 UNITS	88 SPACES	16 ATTACHED GARAGES 16 DRIVEWAY
D BUILDINGS	20 UNITS	100 UNITS	200 SPACES	20 ATTACHED GARAGES 20 DRIVEWAY
E BUILDINGS	12 UNITS	24 UNITS	48 SPACES	8 ATTACHED GARAGES 8 DRIVEWAY
				58 DETACHED GARAGES 296 SURFACE SPACES
TOTAL		240 UNITS	480 SPACES	480 TOTAL SPACES

CONDOMINIUMS (LOT 3)				
	UNITS EACH	TOTAL UNITS	PARKING REQUIRED	PARKING SHOWN
	16 UNITS	112 UNITS	224 SPACES	198 ATTACHED GARAGES (BELOW CONDOS) 63 SURFACE
TOTAL		112 UNITS	224 SPACES	259 TOTAL SPACES

LEGEND

- - - - - EXISTING CONTOUR
- - - - - EXISTING WATERLINE
- - - - - EXISTING TREE LINE
- EXISTING BUILDING
- EXISTING SEWER MANHOLE
- EXISTING H-FRAME POWER POLE
- - - - - PROPERTY BOUNDARY
- - - - - EXISTING FENCE
- - - - - PROPOSED CURB & GUTTER
- - - - - PROPOSED SANITARY SEWER
- PROPOSED CURB INLET
- - - - - PROPOSED WATERLINE
- PROPOSED RIP-RAP EROSION CONTROL
- - - - - PROPOSED CONCRETE WALK

STORMWATER DETENTION DATA

	IA	IIA	QII	Q100
Existing Site	46 ac	.30	75	125 cfs
Developed (66%)	46 ac	.58	85	242 cfs

Required Total Detention Storage = +/- 6 ac-ft
Volume Provided = 12 ac-ft

Civil Engineering
Landscape Architecture
Community Planning
Surveying

Landplan Engineering, P.A.

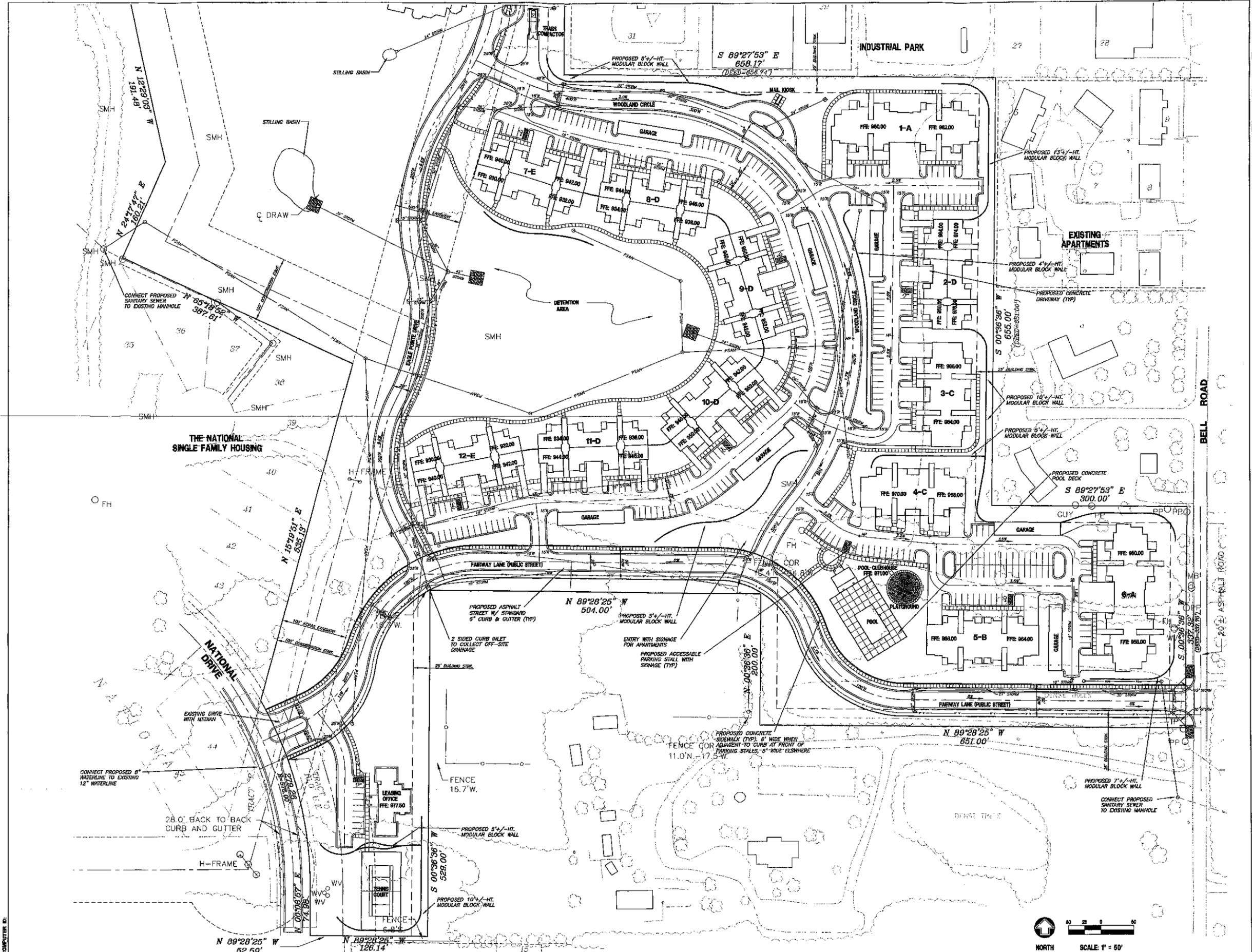
1235 Walnut, Overland Park, MO 66204
1000 Genesee, Suite 400, Kansas City, MO 64102
785-841-6346
Fax: 785-841-6347
www.landplaneng.com

EAGLE POINTE AT THE NATIONAL
APARTMENTS AND CONDOMINIUMS
Parkville, Missouri

SITE PLAN
(GENERAL LAYOUT)

DATE: 8/29/00
PROJECT NO: 2000006
DESIGNED BY: SMF/CAH
DRAWN BY: SMF/CAH
CHECKED BY: JCC

SHEET NO: **1**
1 OF 5 SHEETS



Civil Engineering
 Landscape Architecture
 Community Planning
 Surveying

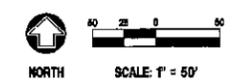
Landplan Engineering, P.A.
 1000 Parkville, Missouri
 Phone: 636-225-1100
 Fax: 636-225-1101
 Website: www.landplan.com

Eagle Pointe at the National
 Apartments and Condominiums
 Site Plan for Apartments
 (Layout)

EAGLE POINTE AT THE NATIONAL
APARTMENTS AND CONDOMINIUMS
 Parkville, Missouri

SITE PLAN
FOR
APARTMENTS
(LAYOUT)

DATE:	8/28/00
PROJECT NO.:	2000/000
DESIGNED BY:	HW/CAW
DRAWN BY:	HW/CAW
CHECKED BY:	JSC



COMPUTER ID:



Civil Engineering
 Landscape Architecture
 Community Planning
 Surveying
 Landplan Engineering, P.A.
 1000 North 1st Street
 Parkville, Missouri 64151
 Phone: (816) 221-1100
 Fax: (816) 221-1101
 www.landplaneng.com

EAGLE POINTE AT THE NATIONAL
APARTMENTS AND CONDOMINIUMS
 Parkville, Missouri

SITE PLAN FOR CONDOMINIUMS (LAYOUT)

NO.	DATE	REVISIONS

DATE: 8/28/10
 PROJECT NO.: 80037D
 DESIGNED BY: BMF/CLM
 DRAWN BY: BMF/CLM
 CHECKED BY: JOC
 SHEET NO.: **3**
 3 OF 6 SHEETS



COMPUTER ID:



Plant Schedule (Apartments & Condominiums)

QTY.	NAME	PLANTED SIZE	CONTR.	MATURE SIZE
44	FRAXINUS PENNSYLVANICA 'SUMMIT' GREEN ASH	2" CAL.	B&B	HT: 40-60' SPREAD: 30-40'
55	QUERCUS ALBISIMA SOUTHERN OAK	2" CAL.	B&B	HT: 40-60' SPREAD: 30-40'
53	QUERCUS COCINIA SCARLET OAK	2 1/2" CAL.	B&B	HT: 50-70' SPREAD: 25-30'
43	KOELBIA PANICULATA 'SEPTENTRIONALIS' GOLDENRAE TREE	1 1/2" CAL.	B&B	HT: 20-30' SPREAD: 20-25'
83	QUERCUS DUNCANII (MALE CULTIVAR) KENTUCKY COFFEE TREE	2" CAL.	B&B	HT: 60' SPREAD: 30'
76	PRUNUS INUSA AUSTRALIAN PINE	6" HT.	B&B	HT: 50-60' SPREAD: 20-30'
66	PICEA PENNSYLVANICA 'ALBERTA' COLORADO BLUE SPRUCE	6" HT.	B&B	HT: 30-40' SPREAD: 15-20'
110	TRILIACIA OCCIDENTALIS 'NIGRA' CANADIAN HELEDOCK	5-6" HT.	B&B	HT: 15-20' SPREAD: 10-15'
92	TRILIACIA OCCIDENTALIS 'AMERICAN ARBORESCENS' AMERICAN ARBORESCENS	5-6" HT.	B&B	HT: 20-30' SPREAD: 10-15'
104	ABELGONIA V. GRANDIFLORA AUTUMN BRILLIANCE GERANIUM	5-6" HT.	B&B	HT: 20-30' SPREAD: 10-15'
105	GERANIUM 'DARWIN' 'DARWIN' KASTORY REDBUD	5-6" HT.	B&B	HT: 15-20' SPREAD: 10-15'
62	MACHONIA HY. 'JANE' LITTLE GIRL HYDRANGEA	5-6" HT.	B&B	HT: 10-15' SPREAD: 10-15'

Civil Engineering
 Landscape Architecture
 Community Planning
 Surveying
Landplan Engineering, P.A.
 100 North Main Street
 Suite 200
 Parkville, Missouri 64151
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EAGLE POINTE AT THE NATIONAL
APARTMENTS AND CONDOMINIUMS
 Parkville, Missouri

SITE PLAN
FOR
CONDOMINIUMS
 (LANDSCAPE)

DATE: 6/25/02
 PROJECT NO.: 0000076
 DESIGNED BY: BME/GAR
 DRAWN BY: BME/GAR
 CHECKED BY: JDC
 SHEET NO.: **5**
 5 of 5 SHEETS

COMPUTER PLT

PRELIMINARY DEVELOPMENT PLAN

NW 1/4, SEC. 26, TWN. 51, RNG. 34

PARKVILLE, PLATTE COUNTY, MISSOURI

LEGEND

○	MONUMENT FOUND	B/L	BUILDING LINE
●	MONUMENT SET	U/E	UTILITY EASEMENT
■	5/8 REBAR SET	—	EX. FENCE
□	EX. STREET SIGN	—	EX. SANITARY SEWER MANHOLE
□	EX. GAS METER	—	EX. TREE LINE
□	EX. GAS ROSEN	○	PROP. WATER VALVE
□	EX. GAS LINE	○	PROP. FIRE HYDRANT
□	EX. WATER METER	—	PROP. WATER LINE
□	EX. WATER VALVE	—	PROP. SANITARY SEWER MANHOLE
□	EX. FIRE HYDRANT	—	PROP. SANITARY SEWER LINE
□	EX. WATER LINE	—	PROP. STORM SEWER JUNCTION BOX
□	EX. STORM SEWER MANHOLE	—	PROP. STORM SEWER CURB INLET
□	EX. STORM SEWER LINE	—	PROP. STORM SEWER PVS
□	EX. STORM SEWER LINE	—	PROP. STORM SEWER LINE
□	EX. SANITARY SEWER LINE	—	PROP. SANITARY SEWER FORCEMAIN
□	EX. POWER POLE	—	PROP. TREE
□	EX. LIGHT POLE	—	MEASURED DISTANCE
□	AIR CONDITIONING UNIT	—	PLATTED DISTANCE
□	EX. ELECTRIC PEDESTAL		
□	EX. OVERHEAD ELECTRIC & NO. LINES		
□	EX. UNDER GROUND ELECTRIC LINE		
□	EX. TELEPHONE PEDESTAL		
□	EX. UNDER GROUND ELECTRIC LINE		
□	EX. CABLE PEDESTAL		

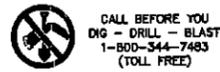


VICINITY MAP
NOT TO SCALE
NW 1/4 SEC. 26-51-34

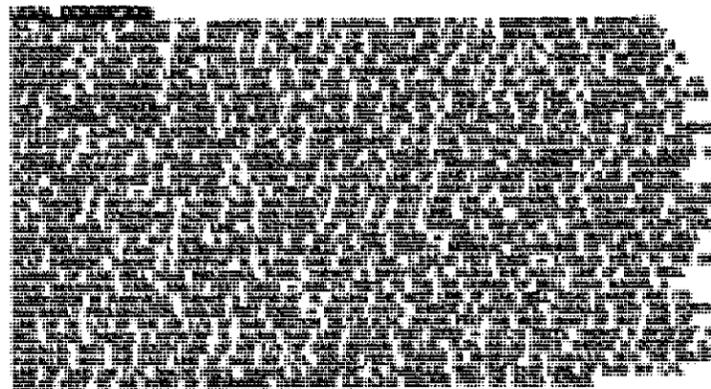
TRACT DESIGNATION TABLE

TRACT	USE	AREA (AC)
A	MULTI-FAMILY	8.60
B	OPEN SPACE	0.46
C	TOWNHOMES	1.29
D	OPEN SPACE	9.08
E	TOWNHOMES	2.64
F	CLUB HOUSE	1.39
G	OPEN SPACE	0.23
H	OPEN SPACE	0.70
I	OPEN SPACE	2.79
J	TOWNHOMES	5.53
K	TOWNHOMES	2.87
L	TOWNHOMES	2.08
M	OPEN SPACE	1.49
N	RIGHT OF WAY	5.68

OPEN SPACE TRACTS WILL BE MAINTAINED BY THE NATIONAL PROPERTIES OWNERS ASSOCIATION.



NOTES:
THIS IS NOT IMPLIED NOR INTENDED TO BE THE COMPLETE INVENTORY OF UTILITIES IN THIS AREA. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF ALL UTILITIES (WHETHER SHOWN OR NOT) AND PROTECT SAID UTILITIES FROM ANY DAMAGE.



FLOOD STATEMENT:
THIS TRACT OF LAND LIES WITHIN ZONE "C" (AREAS OF MINIMAL FLOODING) AS DETERMINED BY FEMA FLOOD INSURANCE RATE MAP NO. 2904475 0185 A, DATED DECEMBER 18, 1979, PLATTE COUNTY, MISSOURI.

LAND OWNER/DEVELOPER:
J3-PAND, LLC
6700 N. NATIONAL DRIVE
PARKVILLE, MISSOURI 64152
CONTACT: TONY BORGHERS/JIM WATSON/JIM ALLEN
TEL: (816) 746-0200 (x342)

PREPARED BY:
KAW VALLEY ENGINEERING
1333 NE. BARRY ROAD
KANSAS CITY, MO 64185
PHONE: (816) 468-8888
FAX: (816) 468-8881

PLAN INFORMATION:
A: EXISTING ZONING = R-5
B: PROPOSED ZONING = R-5 (PLANNED RESIDENTIAL)
C: TOTAL LAND AREA: 46.24 ACRES TOTAL LAND AREA
D: 6.21 ACRES DEDICATED TO PUBLIC RIGHT-OF-WAY
E: NET LAND AREA: 40.03 ACRES NET LAND AREA
F: PROPOSED LAND USE = MULTI-FAMILY
G: BUILDING SUMMARY

BUILDING TYPE	UNITS		GROSS AREA PER FLOOR
	PER FLOOR	PER FLOOR	
MULTI-FAMILY TYPE E	1ST - 8	12	9,652.00
	2ND - 16	24	18,238.80
	3RD - 16	24	18,238.80
	4TH - 16	24	18,238.80
TOTALS	56	84	92,368.70
MULTI-FAMILY TYPE H	1ST - 14	22	13,552.80
	2ND - 22	34	21,387.00
	3RD - 23	36	21,379.50
	4TH - 23	36	21,379.50
	5TH - 15	24	14,908.20
TOTALS	97	152	92,368.70

TOWNHOMES 46 TOTAL BUILDINGS, 2 UNITS PER BUILDING, GROSS FLOOR AREA 215,548 SF
DENSITY PREVIOUS PLAN APPROVAL: 352 UNITS = 7.65 UNITS/AC
THIS PLAN PROPOSED: 245 UNITS = 5.33 UNITS/AC

- H: BUILDING COVERAGE
269,130 SQ. FT. - RATIO 1:0.13
- I: PARKING QUANTITIES
TOTAL 285 SPACES PROVIDED
BLDG E PARKING STALLS REQUIRED = 84
PARKING STALLS PROVIDED = 92
BLDG H PARKING STALLS REQUIRED = 152
PARKING STALLS PROVIDED = 115
CLUB HOUSE PARKING STALLS PROVIDED = 58
- J: COMMENCEMENT AND COMPLETION DATES
K: CONSTRUCTION START SPRING 2006 PHASE I
CONSTRUCTION END SPRING 2008
CONSTRUCTION START SPRING 2006 PHASE II
CONSTRUCTION END SPRING 2010

- L: PRIVATE OPEN SPACE = 14.38 ACRES. PRIVATE OPEN SPACE PLUS CASH IN LIEU OF PUBLIC OPEN SPACE WILL BE ACCEPTED AS PART OF THE TOTAL APPROVED C.U.P.
- M: SEE TRACT DESIGNATION TABLE
- N: CONTOUR INTERVAL = 5' FROM TOPOGRAPHIC SURVEY
- O: SOIL TYPE = SILT CLAY LOAM & VERY FINE SANDY LOAM.
- P: SEWAGE DISPOSAL = PUBLIC - PLATTE COUNTY REGIONAL SEWER DISTRICT AND CITY OF PARKVILLE MISSOURI
- Q: WATER DISTRICT - MISSOURI AMERICAN WATER
- R: ELECTRIC - KANSAS CITY POWER & LIGHT
- S: GAS - MISSOURI GAS ENERGY
- T: ALL CURB WILL BE CG-2
- U: ALL SIGNS WILL BE BUILT PER CITY DETAILS.
- V: TOWNHOMES WILL HAVE DUEL MAILBOXES PLACED BETWEEN DRIVEWAYS.

BENCHMARK:

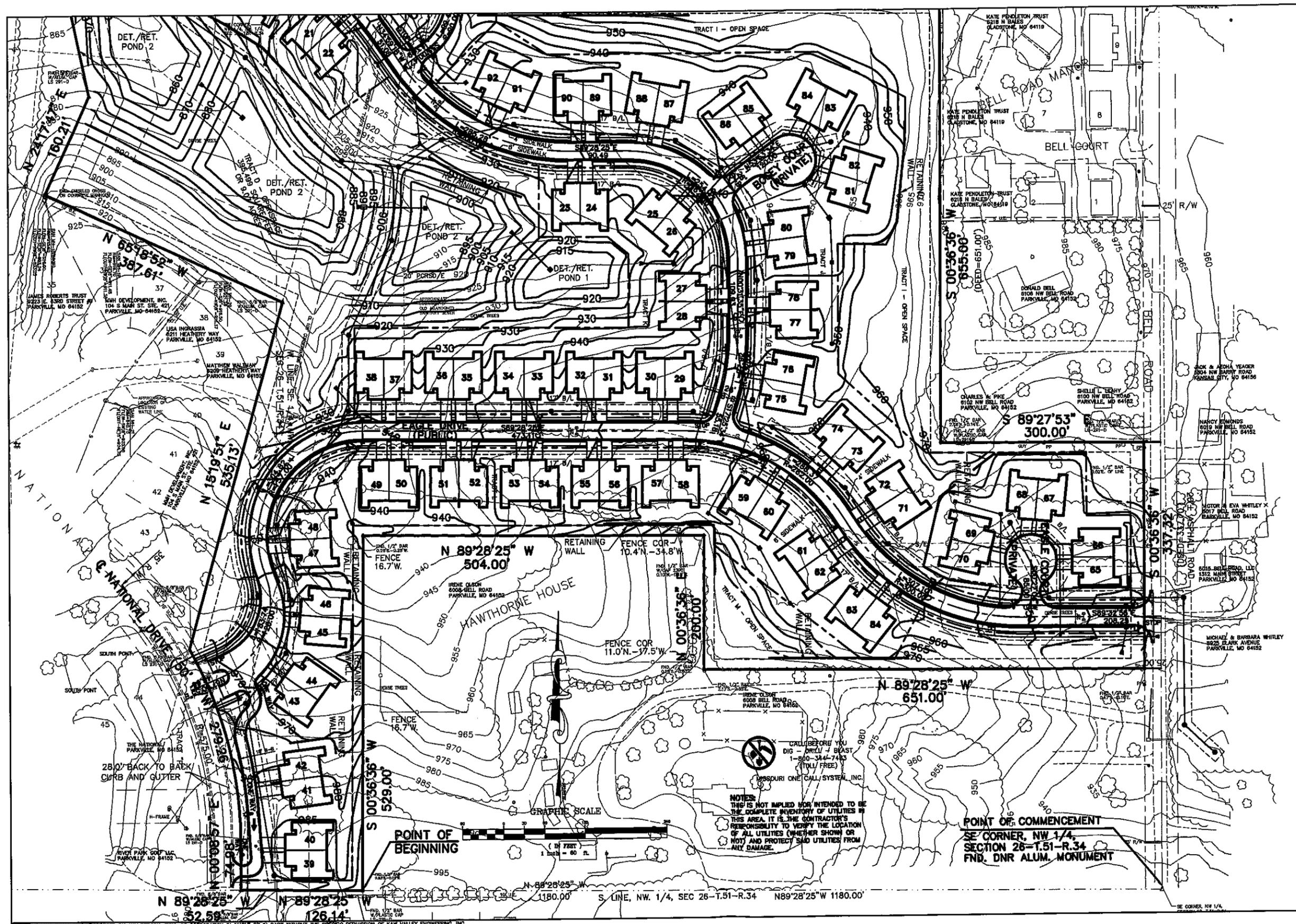
BM#1 = CHISELED SQUARE AT THE SOUTHEAST CORNER HEADWALL ± 350' WEST OF THE NORTHWEST CORNER OF PROPERTY.
ELEV.: = 833.92'
BM#2 = S. BORNET BOLT ON FIRE HYDRANT NORTHSIDE OF FIRE HYDRANT 100' ± EAST OF THE NORTHWEST PROPERTY CORNER.
ELEV.: = 833.90'

NOTES:

- NO SITE GRADING SHALL BE DONE WITHOUT A GRADING PERMIT BEING ISSUED PRIOR TO CONSTRUCTION.
- ALL STREET GRADES SHALL BE DESIGNED PER ARTICLE II, SECTION 506.009 OF THE PARKVILLE MUNICIPAL CODE. NO STREET PLANS SHALL BE CONSTRUCTED PRIOR TO ENGINEERING PLANS BEING APPROVED BY PARKVILLE PUBLIC WORKS DIRECTOR AND STREET MAINTENANCE BONDS BEING RECEIVED AND APPROVED BY THE BOARD OF ALDERMEN. ALL PROPOSED STREETS ARE LOCAL STREETS UNLESS OTHERWISE SHOWN.
- PERSD EASEMENTS (PERSD/E) ARE DEDICATED TO PLATTE COUNTY REGIONAL SEWER DISTRICT FOR THEIR SEWER MAINS. (20' WIDTH). S/E ARE DEDICATED TO CITY OF PARKVILLE FOR THEIR SEWER MAINS. (20' WIDTH)

	CHK
	DSK/DWG
	DATE
	REV
 KAW VALLEY ENGINEERING, INC. - CONSULTING ENGINEERS <small>Office: Kansas City, Mo. Kansas City, Mo. License No. 0000000000</small>	1333 NE BARRY ROAD KANSAS CITY, MISSOURI 64185 816-468-8888 FAX 816-468-8881 E-MAIL: kve@kve.com WEB SITE: www.kve.com
THE VILLAS AT THE NATIONAL MO HIGHWAY NO. 45 & LAKE CREST LANE PARKVILLE, MISSOURI	TITLE SHEET
PROJ. NO. 80000779	
DATE 04/15/06	
DESIGNER GDC	
DRAWN BY MCB	
CHK 07780P	
SHEET 1 of 3	0

\\Drummond\DWG\B05_0776\DESIGN\0776PDP.dwg, 8/18/2005 1:35:43 PM, Drummond



REV	DATE	DESCRIPTION

<p>1333 N.W. BARRY ROAD LAWRENCE, MISSOURI 64105 816-842-2200 FAX 816-842-0861 WWW.KAWVALLEYENGINEERS.COM</p>	<p>KAW VALLEY ENGINEERS, INC. - CONSULTING ENGINEERS 1333 N.W. BARRY ROAD LAWRENCE, MISSOURI 64105</p>
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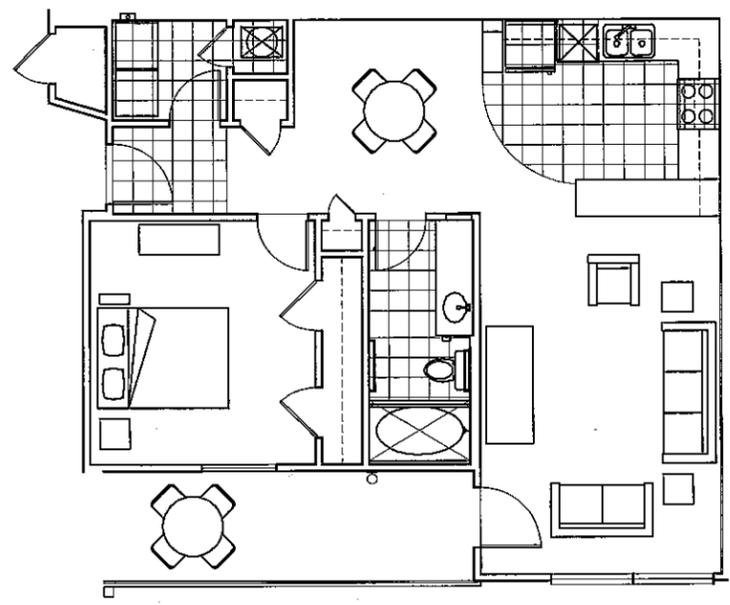
<p>THE VILLAS AT THE NATIONAL MO HIGHWAY NO. 45 & LAKE CREST LANE PARKVILLE, MISSOURI</p>	<p>PRELIMINARY DEVELOPMENT PLAN</p>
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<p>PROJECT NO. 80500776 DATE 04/15/05 DESIGNER GBC DRAWN BY MSB CDR 0776PDP SHEET 3 of 3</p>	<p>0</p>
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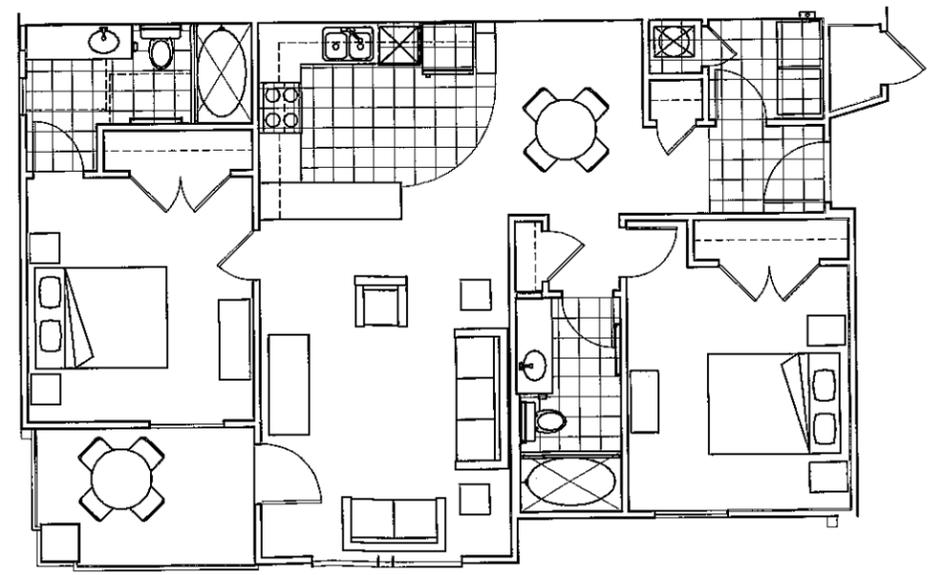
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**BUILDING H
REAR ELEVATION**
1/8" = 1'-0"



UNIT A
1/4" = 1'-0"



UNIT B
1/4" = 1'-0"

GENERAL NOTES	
1 ASPHALT SHINGLES	
2 FIBER CEMENT LAP SIDING	
3 STUCCO	
4 FAUX STONE	

JOB NUMBER	06013
ISSUE DATE	03/18/08
REVISIONS	

Williams Spurgeon
Kuhl & Freshnock
Architects, Inc. © 2008

THE NATIONAL GOLF CLUB OF KANSAS CITY
PHASE FOUR
 PARKVILLE, MISSOURI

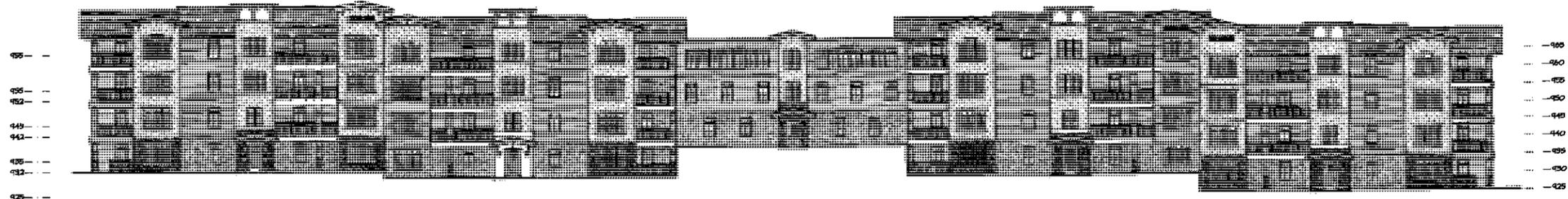
Williams
Spurgeon
Kuhl &
Freshnock
Architects, Inc.

110 Armour Road • North Kansas City, Missouri 64116 • Tel. 816.350.4101 • Fax 816.350.4102

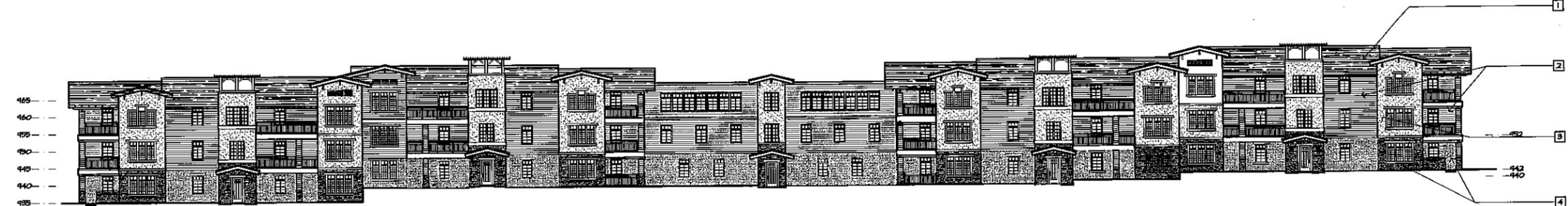
**BUILDING E
ELEVATIONS**
A2.01

JOB NUMBER
- 06018 -
ISSUE DATE
- 08 / 09 / 08 -
REVISIONS

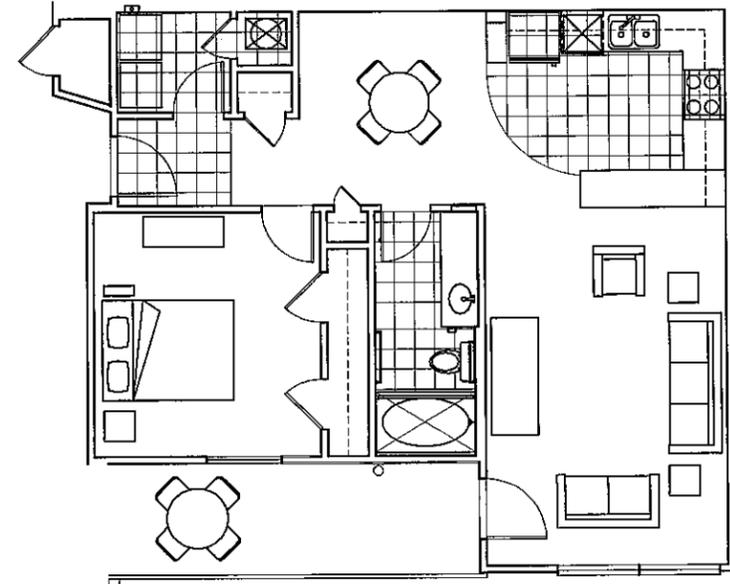
Williams Spurgeon
Kuhl & Freshnock
Architects, Inc. © 2008



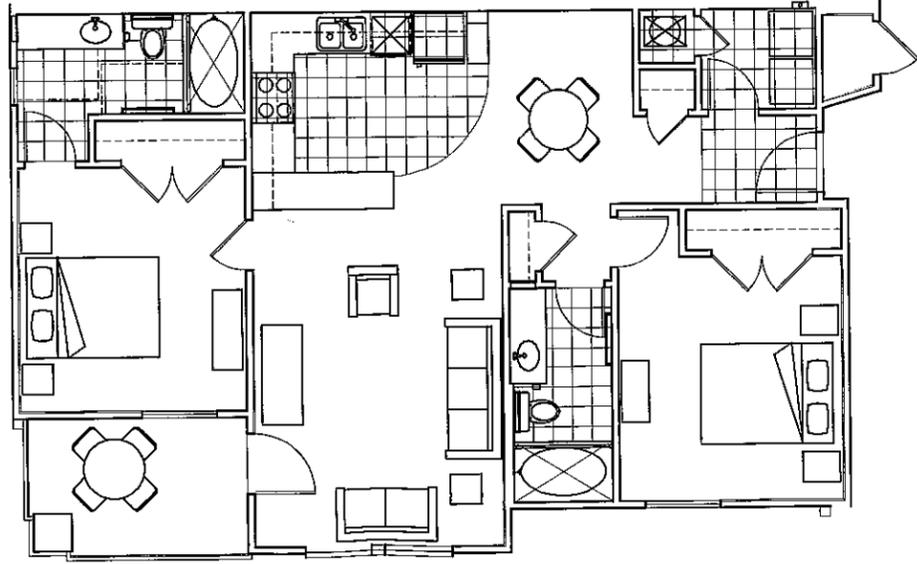
**BUILDING E
REAR ELEVATION**
1/8" = 1'-0"



**BUILDING E
FRONT ELEVATION**
1/8" = 1'-0"



UNIT A
1/4" = 1'-0"



UNIT B
1/4" = 1'-0"

GENERAL NOTES	
1 ASPHALT SHINGLES	
2 FIBER CEMENT LAP SIDING	
3 STUCCO	
4 FAUX STONE	

THE NATIONAL GOLF CLUB OF KANSAS CITY
PHASE FOUR
 PARKVILLE, MISSOURI

Williams
Spurgeon
Kuhl &
Freshnock
Architects, Inc.

10 Armour Road - North Kansas City, Missouri 64116 - Tel. 913.300.4181 - Fax 913.300.4182

**BUILDING H
ELEVATIONS**
A2.01