

Staff Analysis

- Agenda Item: **4.B**
- Proposal: **Application for preliminary site plan approval for the Lake Point Lodge - an apartment building, club house and associated improvements on 5.02 acres, more or less.**
- Case No: PZ15-03
- Applicant: KGH Building Group LLC
- Owners: SKG, LLC
- Parcel # (s): 20-7.0-26-100-003-012.001, 20-7.0-26-100-003-012.002 and 20-7.0-26-100-003-013.000.
- Exhibits:
- A. This staff report
 - B. Application for Site Plan / Development Plan
 - C. March 25, 2015 Preliminary Site Plan drawing packet submitted by NSPJ Architects and including Architectural drawings A0.00 through A3.01 (10 sheets), Landscape drawings L1.00 and L2.00, Civil drawings C1.0 through C3.0 (3 sheets) and Lighting drawings E1.0 and E2.0
 - D. Four drawings provided by the applicant comparing the proposed apartment building and previously approved office buildings (undated)
 - E. Two elevation provided by the applicant drawings simulating views to the proposed apartment buildings from the nearest neighbor to the northeast and 9 Highway to the southeast (undated)
 - F. Pages 1 through 6 of the Micro Stormwater Drainage Study, Lake Pointe Lodge, Prepared By David Eickman, PE, Olsson Associates and last dated March 6, 2015
 - G. Section 6.0 Recommendations & Conclusions (report pages 24 and 25) from the Traffic Impact Study for Parkville Apartments, Missouri Highway 9 & Clark Avenue signed and sealed by Todd A Fredericksen, PE, of Olsson Associates on March 4, 2015
 - H. March 27, 2015 memo from Public Works Director Alysen Abel
 - I. February 24, 2015 review letter from Paul M. Bertrand, P.E., PTOE, George Butler Associates, Inc. to Public Work Director Alysen Abel regarding independent review of the proposed traffic study
 - J. Trip generation table by land use code
 - K. Review comments dated 3-6-15 from the Southern Platte Fire Protection District Fire Marshal Dean Cull
- By Reference: A. *"R-5" Planned Multi-Family Residential District Regulations -*
<http://ecode360.com/27901260>

- B. "B-4" Planned Business District Regulations - <http://ecode360.com/27901710>
- C. Parkville zoning code in its entirety - <http://www.ecode360.com/PA3395-DIV-05>
- D. Parkville Master Plan - <http://parkvillemo.gov/departments/community-development-department/master-plan/>
- E. Traffic Impact Study for Parkville Apartments, Missouri Highway 9 & Clark Avenue signed and sealed by Todd A Fredericksen, PE, of Olsson Associates on March 4, 2015 - <http://parkvillemo.gov/wp-content/uploads/2015/03/SEALED-scanned-FINAL-Apartment-Parkville-Report-3.4.15.pdf>
- F. Micro Stormwater Drainage Study, Lake Pointe Lodge, Prepared By David Eickman, PE, Olsson Associates and last dated March 6, 2015 - http://parkvillemo.gov/wp-content/uploads/2015/03/Stormwater-Study_Rev-2.pdf

Public Comments

Received:

- A. Email received 2-26-15 from Vicki Raine, 8508 NW 62nd St., Parkville
- B. Letter received 3-3-15 from Don Julian, President, Riss Lake Homeowners Association
- C. Email received 3-23-15 from Robar Mike and Junetta Duncan, 8409 NW 62nd St., Parkville
- D. Letter received 3-27-15 from Harry Sievers, 6508 NW Melody Lane, Parkville
- E. Email received 3-27-15 from Tami Jordan, 6113 NW Pine Ridge Rd, Parkville
- F. Email received 3-27-15 from Mary Ann Lober, 6100 NW Pine Ridge Rd, Parkville
- G. See also minutes of the associated March 10, 2015 public hearing before the Planning and Zoning Commission.

Associated

Application:

Agenda Item 4A, application to rezone 5.02 acres to "R-5" zoning

Overview

The application proposes a preliminary site plan for development of a multi-story, 50-unit apartment building, a separate clubhouse and pool, a monument sign, parking in attached garages, carports and uncovered stalls, landscaping / screening and other proposed improvements on 5.02 acres, more or less. The property is located on the east side of 9 Highway, east of the Clark Avenue (east of Mosaic, the Global Orphan Project, Craig Marshal Dental building and the Southern Platte County Community Center / YMCA) and are identified as Lots 1 and 2 of the Final Plat, Lake Pointe Professional Centre, A Part of the Northeast ¼, Section 26, Township 51 North, Range 34 West, Parkville, Platte County, Missouri. The properties are also identified as Platte County parcel numbers: 20-7.0-26-100-003-012.001, 20-7.0-26-100-003-012.002, and 20-7.0-26-100-003-013.000.

The application also proposes public improvements, including removal of an existing single-family home, re-grading, and construction of a new trail, on the abutting City-owned parkland to

the north (Lot 3 of the Final Plat, Lake Pointe Professional Center, also known as Platte County parcel number 20-7.0-26-100-003-012.000).



Specifically, the preliminary site plan proposes a five-story, 50 unit apartment building containing four floors of residences over garages and storage on the lowest floor on 5.02 acres, more or less, owned by the applicant. The building contains 17 one-bedroom units (with dens) and 33 two-bedroom units (3 with dens), all with private laundry facilities. The building provides optional storage units and 14 enclosed parking garages. 85 parking spaces are provided (including the enclosed garages and 30 covered parking stalls), exceeding the City's minimum requirements by 10 spaces. The project also includes a clubhouse with development offices, a reception area, exercise room, game hall, social hall, pool, restrooms and other amenities with limited hours. The building is proposed to be clad with stone veneer, stucco, painted cementitious siding (mimicking cedar shingles), standing seam metal roofs on the building corners and asphalt shingles on the remainder of the roof, aluminum balcony railing and exposed heavy timber trusses, giving the appearance of a lodge and incorporating several design features similar to those on the apartments at Burlington Creek east of Parkville. The clubhouse uses the same materials pallet. The building is proposed to be sprinklered in accordance with the applicable 2012 Building and Fire codes.

In addition, the applicant proposes improvements on the abutting property to the north owned by the City of Parkville. The property was previously part of a development plan approved for the "Lake Pointe Professional Centre." The development plan included two 28,000 square foot buildings, 203 parking spaces and an open space tract. Following approval of rezoning and a site plan in 2003 and preliminary and final plat in 2004, the open space tract property was deeded to the City by the applicant in November 2005. The property was to serve as a buffer to the abutting single-family residential development and was later zoned "PCLD" Parkland and Conservation District to help ensure preservation. The vacant single-family home on the property was to be demolished and the site regraded with the development of the approved office buildings. Since the project never developed the building has never been demolished. As part of the current application, the applicant has agreed to demolish the structure, regrade and reseed the site and to provide a trail through the site providing access between 62nd Street, 9 Highway and the development.

The applicant is requesting the City count the previously dedicated parkland toward current parkland requirements and toward the overall acreage of the development for the purpose of calculating density. The applicant submits that the property was previously dedicated in good faith as part of an approved site development plan and therefore should be counted toward the proposed development. Unless credited toward the current parkland dedication requirements and density calculations, an additional 0.6 acres of parkland would have to be dedicated and the apartment unit count would have to be reduced to 40 units. The requested credit is beyond the authority of the Planning and Zoning Commission and would be required to be approved by the Board of Aldermen, unless the Board deeds the property back to the applicant as part of the consideration. Any approval of the site plan would have to be contingent on Board approval of the requested credits unless the plans are recommended for only 40 units with additional parkland dedication.

The preliminary site plan is proposed in conjunction with Case PZ15-02, an application to rezone 5.02 acres, more or less, (parcels 20-7.0-26-100-003-012.001, 20-7.0-26-100-003-012.002 and 20-7.0-26-100-003-013.000) from “B-4” Planned Business District to “R-5” Planned Multi-Family Residential District.

The application was previously submitted with an application for rezoning to “R-4” Multiple-Family Residential District, and discussed as part of a public hearing held before the Planning and Zoning Commission on March 10, 2015. The site plan has since been revised to address some staff review comments, and comments and questions from the Planning and Zoning Commission and public that spoke during the meeting. Primary changes include:

- moving the west parking lot approximately 10 feet closer to the building, addressing fire access comments and providing increase separation from 9 Highway – increasing the average setback between the western retaining wall and the 9 Highway right-of-way to 29 feet with 45 feet to the edge of pavement (the closest point would be the retaining wall around the cul-de-sac bulb at 15 feet from the right-of-way and 31 feet from the edge of pavement);
- terracing the retaining wall to create two tiers with landscaped planting beds between;
- the addition of rolled back or mountable curbs and a paved apron to accommodate 30-foot trucks in the cul-de-sac; and
- submittal of photometric plans and lighting details.

See the March 25, 2015 memo from Michael Knapp of NSPJ Architects and the revised architectural drawings from NSPJ Architects dated March 25, 2015 which highlights these and other architectural and site changes.

Review and Analysis

The site is currently zoned “B-4” Planned Business District. Apartments are not a permitted use in the B-4 district, so the preliminary site plan application is submitted in conjunction with an application for “R-5” Planned Multi-Family Residential District zoning. Chapter 426, “R-5” *Planned Multi-Family Residential District Regulations* permits “multiple family dwellings and apartments” subject to approval of a site plan by the Board of Aldermen. The application has been reviewed for compliance with all requirements of this Chapter as well as Chapter 460 *Vehicle Parking*, Chapter 463 *Sign Code* and all other applicable zoning regulations. With the exception of the following, the application meets or exceeds all applicable zoning and subdivision requirements.

1. **Building height** – There is no maximum height or number of stories specified for the R-5 zoning district. Instead, Section 426.040, Height, Area and Bulk Regulations, states “the

height, area, and bulk requirements shall be set by the Board of Aldermen.” A primary consideration for height is compatibility with heights of other structures in the area, visibility from the surrounding properties and comparison to the heights of nearby development. The site plans propose a five-story building terraced down the hillside. The height of the building’s west facade is 46 feet from the elevation of the parking lot to the highest roof peak. The site design takes advantage of the elevation changes with only three stories of the building visible from the east side of the development. The 3rd story (lowest story visible on the east side of the building) would be located approximately 15 feet below the elevation of 9 Highway, leaving only 31 feet visible to the west. Due to the site terracing, the visible elevation of the north façade would be four stories at 49 feet and the south façade would be four stories at approximately 50 to 52 feet. The building’s east façade would be nearly 64 feet tall. However, due to proposed tree preservation and the elevation of the site in comparison to abutting sights, much of the north, south and east elevations would be screened from public view. Where visible, the building would appear shorter since the lower three floors are below the elevation of the abutting roads and private properties.

In comparison the nearest buildings to the west are two- to three-story office buildings with heights up to 45 feet. The properties to the west are zoned “B-4” Planned Business District which has no maximum height restriction. The nearest buildings to the north, south and east are one- and two-story single-family structures with heights allowed up to 35 feet or 2.5 stories.

- 2. Utilities - service and easements** – As of this report, Missouri American Water has not confirmed that they can provide adequate services for the project. Any approval of the plan would require confirmation that adequate service can be provided. The City of Parkville has confirmed that sanitary sewer needs can be met if the existing pumps in the Pinecrest pump station are replaced with larger pumps. The City previously budgeted to replace the existing pumps as part of the adopted capital improvement plan and the applicant has agreed to pay for any increase in cost for increasing the size of the pumps. MGE has verified that gas is available on the west side of 9 Highway and could be extended to the site. KCP&L has verified that three-phase power is available on the east side of 9-Highway and could be extended to the site. In both cases, gas and electrical plans would be required to be reviewed and approved prior to approval of any final site plan. In addition, it needs to be determined if any existing power lines along the 9 Highway frontage or cutting through the site will be relocated or placed underground and whether additional easements will be required. With regard to water, service needs to be coordinate with the Southern Platte Fire Protection District to ensure adequate water flow and pressure for fire suppression.

Site easements also need to be finalized. At a minimum the existing access easement to the City’s Pinecrest pump station (located southeast of the site) needs to be vacated and new access following the entrance and internal driveways must be dedicated. Both are shown on the revised civil plans but would be required to be addressed with a revised plat or by separate instrument. Other easement vacations and dedications may also be required as part of any utility and final plan approvals.

- 3. Right-of-way** – The site proposes direct access to 9 Highway which is owned and maintained by MoDOT. Any plan approval must be subject to verification of access permits and any right-of-way dedication required by MoDOT. A primary consideration is the potential need to widen 9 Highway in the future. The City has recently received a grant to fund a conceptual design necessary to determine the future layout of 9 Highway and needed rights-of-way. Since the project is not yet underway, staff cannot determine if additional

right-of-way should be dedicated along the 9 Highway frontage. Instead, staff has discussed the potential need with the applicant. In order to provide greater separation from 9 Highway and address fire safety code requirements, the west parking lot was moved 10 feet closer to the building. This relocation increased the average setback between the western retaining wall and the 9 Highway right-of-way to 29 feet with 45 feet to the edge of pavement (the closest point would be the retaining wall around the cul-de-sac bulb at 15 feet from the right-of-way and 31 feet from the edge of pavement). The existing B-4 zoning and proposed R-5 zoning have no required setbacks. Instead they are determined by plan approval. The most comparable zoning district to that proposed with specified setbacks is the "R-4" Multiple-Family Residential District which requires a 15 foot setback. The proposed plans would meet this setback. The most comparable non-residential district to the existing B-4 zoning which specifies setbacks is the "B-2" General Commercial District which requires no front-yard setbacks.

The revised preliminary plans have been submitted to the City's hired traffic consultant to help evaluate the potential to accommodate the potential need to widen 9 Highway in this location. As of the time of this report, the setbacks are still being evaluated. Following the last meeting, members of the Commission and public enquired as to whether consideration of the development should be deferred until after the 9 Highway study is completed. Unless a moratorium is approved by the City, the City would have no authority to prevent consideration of an application in this manner. No such moratorium was approved prior to submittal of the application.

4. **Exterior lighting** – An exterior lighting plan has been submitted with the revised plans (see sheets E1.0 and E2.0). The plans show pole lights, bollard lights and building mounted exterior lights. The pole lights have recessed lenses to minimize glare and light spillover onto abutting properties. The plans show that the lighting does not spillover onto abutting properties or the public right-of-way (0.0 foot candles at the property lines). Plan approval should be subject to final lighting plan approval and inclusion of all final exterior building lighting. This is particularly important as it relates to glare for traffic on 9 Highway and light spillover on to the abutting residential properties.
5. **Clubhouse and pool hours of operation** – The preliminary site plan indicates that the clubhouse would be open 24 hours a day, but that the pool will be restricted to hours between 8:00 am and 10:00 pm. Beyond the restricted hours for the pool, staff recommends the outdoor patio and any outdoor activities at the clubhouse are restricted to the same hours except for limited special occasions with advance notice to abutting property owners.

Impacts from neighborhood clubhouses and pools were recently evaluated with the consideration of a new pool in the National Golf Club development. Staff found that pools in River Hills, Thousand Oaks (66th and Nevada) and Riss Lake (on Deer Run) had hours generally from 9:00 am to 9:00 pm and had setbacks of 10 to 70 feet to the nearest abutting residential structure with changes in elevation and landscaping providing buffering and use of restricted hours to minimize impacts. Staff did not find significant impacts to the abutting properties resulting from activities and operations in the locations. In comparison to these locations, the proposed pool would be partially screened by the proposed clubhouse and would be approximately 235 feet from the nearest residence to the north and over 300 feet to the nearest residence to the east with existing vegetation separating the pool from all surrounding sites.

6. **Trash collection** – Several comments have been received regarding trash collection and related noise. Trash collection is regulated by Parkville Municipal Code Section 225.030, *Collection of Solid Waste*. This section prohibits collection of trash between the hours of 10:00 pm and 6:00 am. The applicant has considered relocation of the dumpsters to the south side of the site, but concluded that the necessary trash truck access cannot be accommodated. Instead, the applicant suggested that trash collection would likely be scheduled for mid-day pickup to avoid conflicts with morning residential traffic in the site. Beyond that dumping trash in the dumpster can be regulated to limited hours so apartment residents and nearby single-family residents would not be impacted by unnecessary noise.
7. **Retaining walls and guard rails** – The preliminary site plans have been modified to revise the western most retaining wall. The wall is approximately 13 feet tall and has been modified to include two tiers with a landscaping bed between the two wall sections. A 3' 6" guard rail is also shown along the top of the fence. A note has been added to clarify that the rail will look similar to black aluminum railing proposed for the balconies. Landscaping is also proposed along the back side of the proposed sidewalk along 9 Highway. The vegetation is intended to improve the appearance along the 9 Highway frontage but would also help create an additional buffer between vehicular traffic on 9 Highway and the 13 foot drop to the west parking lot.

The plans note that the final retaining wall locations and details would be subject to final grading plans and structural engineering considerations. The applicant previously testified that the retaining walls would be constructed of concrete masonry unit or MSE block with shorter walls made of limestone. To ensure similar quality and character, while providing flexibility for final design, staff recommends any approval be subject to approval of final engineering and site plans with materials being substantially the same unless otherwise approved by the and Board of Aldermen following review and recommendation by the Planning and Zoning Commission. Final details must include fences or railing along the top of retaining walls with a height greater than 30 inches and located near pedestrian walkways. Final materials details for the ornamental guardrail along the top of the retaining wall closest to 9 Highway should also be required. This guardrail is particularly important due to the close proximity to 9 Highway and needs to be functional and attractive.

Prior to any final approval, the developer's engineer shall also submit structural calculations and plans concurrent with the construction plan submittal and in accordance with the City's adopted regulations. Where landscaping in close proximity to the retaining walls is shown staff recommends, use of columnar plantings or use of root barriers to minimize any long-term impacts to the walls from the tree roots.

8. **Fire safety** – The application has been reviewed by the Southern Platte Fire Protection District (SPFPD) staff. SPFPD has identified the need to access the site with their largest vehicle, a 40-foot ladder truck. The plans were revised to verify the 40 foot ladder truck can be circulated within the site as required by the adopted Fire Code.

SPFPD has previously recommended that all roads and driveways providing access to the fire department be widened to 26 feet minimum, that roll-back or mountable curbs be used and that the separation between the building and the front parking lot not exceed 30 feet to allow adequate access with ladders and that the separation be no less than 15 feet for fire safety. The plans were revised to move the west parking lot closer to the building to meet the 30 foot maximum separation, rollback or mountable curbs are called out and an apron

has been added behind the curb in the cul-de-sac to allow greater turning radius and accommodate over steering.

As of the time of this report, final comments from SPFPD have not been received. Any approval will be subject to final SPFPD approval.

9. **Drive slopes** – The revised plans verify that all drive slopes are less than the City's maximum allowable slope of 15%. Proposed slopes range from slightly less than 6% to nearly 14%. Moving the west parking lot closer to the building did not change the drive slopes within the site. It did however reduce the slope on the area between the sidewalk and westernmost retaining wall.

Beyond minimum zoning and subdivision regulation standards staff recommends the following conditions:

1. **Landscaping** – The landscape plan was revised to increase the turf areas and reduce the native plantings. This change impacts the stormwater calculations. Public Works Director Alysen Abel has reviewed the revised stormwater study and concluded that the plans can be modified to increase the required Level of Service to compensate for reduction of the native plantings with details to be worked out with construction plan submittal. Approval of the preliminary plan should be contingent on the details of the landscape plan and stormwater plans coordinating and all associated requirements being met.
2. **Improvements to parkland** – The application shows demolition and removal of the existing single-family home on the City owned property to the north of the subject site. The plans also show a trail to be constructed by the applicant across the City's property. Approval of the preliminary development plan should be subject to final grading plans for these improvements, modification of the landscaping plans to show any disturbed area being reseeded or sodded, trail or sidewalk details and other associated construction details. Approval should also be subject to execution of a development agreement addressing maintenance of the improvements.
3. **Stormwater** – Staff has reviewed preliminary plans and concluded that minimum standards and customary engineering requirements can be met. See review stormwater review comments in Public Works Director Alysen Abel's March 27, 2015 memo. Concurrent with the submittal of construction plans, the developer's engineer shall provide a Final Stormwater Management Study that contains final design calculations for the stormwater system. The study shall be approved prior to issuance of a permit. The applicant's engineer shall submit detailed drawings and engineering calculations associated with the stormwater detention and stormwater treatment facilities. Drawings and calculations shall be approved prior to issuance of a grading permit. At the time of construction plan review, the Public Works staff will review the need for additional agreements, easements, and bonds associated with the construction of the stormwater detention and stormwater treatment facilities.

Public Works Director Abel recommends approval subject to the following conditions:

- a. Concurrent with the submittal of construction plans, the developer's engineer shall provide a Final Stormwater Management Study that contains final design calculations for the stormwater system. The study shall be approved prior to issuance of a permit.

- b. Following the rezoning approval, the City staff will work directly with the developer's engineer with the Stormwater Treatment Facility plan to increase the level of service to compensate for the reduction in native vegetation.
 - d. The applicant's engineer shall submit detailed drawings and engineering calculations associated with the stormwater detention and stormwater treatment facilities. Drawings and calculations shall be approved prior to issuance of a grading permit.
 - e. At the time of construction plan review, the Public Works staff will review the need for additional agreements, easements, and bonds associated with the construction of the stormwater detention and stormwater treatment facilities.
4. **Erosion Control** - Staff has reviewed preliminary plans and concluded that minimum standards and customary engineering requirements can be met. See erosion control review comments in Public Works Director Alysén Abel's March 27, 2015 memo. Public Works Director Abel recommends approval subject to the developer's engineer providing erosion and sediment control plans in accordance with APWA 5100 design standards. Additional measures may be deemed necessary by City staff upon review. Practices such as a sediment basin or sediment trap may be evaluated at the time of construction plan review. Turf reinforcement mat shall be placed on any slopes of 3:1 or greater.
5. **Traffic** – Staff has reviewed preliminary plans and concluded that minimum standards and customary traffic engineering requirements can be met. See traffic review comments in Public Works Director Alysén Abel's March 27, 2015 memo and the February 24, 2015 review letter from Paul M. Bertrand, P.E., PTOE, George Butler Associates, Inc. to Public Work Director Alysén Abel regarding independent review of the proposed traffic study. Public works Director Abel recommends the preliminary plans be approved subject to the following:
- a. Update the pavement marking in the dual turn lane on 9 Hwy. Methods for pavement marking and removal of existing pavement marking shall be included in the construction plans.
 - b. Provide two exiting lanes from the development, one thru/right lane and one left turn lane with a minimum of 40 feet of storage in the left turn lane.
 - c. Convert the existing eastbound right turn lane on Clark Avenue on the west side of 9 Hwy to a thru/right lane. Methods for pavement marking and removal of existing pavement marking shall be included in the construction plans.
 - d. Final approval of the parking lot layout and entry drive.
6. **Sewer service and the Pinecrest pump station** – Staff has reviewed preliminary plans and concluded that sewer service can be provided and that all sanitary sewer improvement requirements can be met with required upgrades. See wastewater review comments in Public Works Director Alysén Abel's March 27, 2015 memo. Public Works Director Abel recommends approval of the preliminary plans subject to the following requirements:
- a. The developer shall reimburse the City for the cost difference of installing a pump with larger capacity to handle the proposed development.

- b. The developer shall provide a revised access easement for the City to access the Pinecrest Pump Station.
- c. The contractor shall provide access for the City personnel to the Pinecrest Pump Station during construction.
- d. The developer shall provide a security gate for the City personnel to access the driveway to the Pinecrest Pump Station.
- e. The developer shall provide a fence surrounding the existing Pinecrest Pump Station. The location and fencing materials can be determined at the time of construction plan submittal.
- f. The developer's engineer shall update the grading plan to include a reduced grade on the entry drive to the pump station and include ditches on either side of the drive to prevent stormwater issues.

The plan was previously evaluate against Parkville Municipal Code Section 478.070, *Standard of Review*, which applied to site plans in the R-4 zoning district. If the plans are approved under the R-5 zoning as requested, these standards of review are no longer applicable. However, these standards are good guides for the consideration of a site plan and help address many of the concerns heard to date. These standards are also the same or similar to matters recommended to be considered for rezoning so many of the conclusions drawn herein are drawn from the conclusions on the associated rezoning application. Again staff offers the following comments as a guide only. Under the R-5 zoning these standards are not required to be met.

1. **The extent to which the proposal conforms to the City's Zoning Code.** Staff concludes that if the property is rezoned to allow multiple family residential uses, the plan submitted is or can conform to all applicable requirements of the City's zoning code. See above for details.
2. **The extent to which the proposal conforms to the provisions of the City's subdivision regulations.** Staff concludes that if the property is rezoned, the plan submitted is or can be consistent with all requirements of the applicable sections of the City's subdivision regulations. On December 20, 2005 the Board of Aldermen approved the Final Plat, Lake Pointe Professional Centre containing all properties proposed to be developed. The plat was approved as compliant with all applicable subdivision regulations in place at that time. The plat took full effect when recorded in the Platte County Recorder of Deed's office on December 28, 2005 as instrument 24204 recorded in book 20, page 129.

Only minor revisions have been made to the subdivision regulations since that time and none appear to affect the previously approved plat. As a result of any site plan approval, easements recorded with the plat would have to be abandoned and new easements would have to be recorded by separate instruments. The applicant may also want to re-plat Lots 1 and 2 into one lot as opposed to the two lots previously created for two separate office buildings.

3. **The extent to which the proposal conforms to the goals and objectives of the City's adopted Master Plan.** The adopted Master Plan projects the subject property for *Office/Business Park* uses. This projection is intended for office, research, limited retail, and

limited flex industrial (light industrial) uses in an office park setting. These areas are intended to promote the economic viability of employment uses. This category is not considered appropriate for “heavier” intensity or industrial uses such as manufacturing, outdoor storage, warehousing, distribution, wholesale, and other similar industrial uses. This category does not include any projection for residential uses. This projection was primarily based on the existing B-4 zoning and the Lake Pointe Center office development approved at that time.

The proposed apartment development is not consistent with this projection, but could accomplish similar goals and objectives from the Master Plan. The proposed apartments can only be approved with an “R-5” Planned Multi-Family Residential District zoning. The R-5 zoning, if approved, would be most compatible with the *Moderate Density Residential Mix* projection. This projection is intended for single-family development, supported by a variety of other building types where appropriate, at densities up to eight units per acre. Small lot or cluster single-family development, and attached residential development primarily consisting of two-unit houses, townhomes, multiplexes, and garden apartment/condo dwellings may be intermixed within the single-family development to provide for greater housing diversity and choice. These areas are intended to be master planned developments to ensure compatibility, long-term value, and connectivity between housing types, in accordance with the Neighborhood Design Policies and Design Expectations. More intense residential development is intended for a Mixed Use District. This projection is a transition between the lower density Residential Neighborhood projection (projected for Pinecrest and Riss Lake) and the higher density Mixed Use Residential Neighborhood category (projected for areas with a higher mix of residential and non-residential uses).

The Master Plan promotes infill development and mixed-use development as a means of maximizing existing infrastructure, providing greater housing choices to meet broader needs than met with single-family housing and providing a transition between residential and non-residential uses. The proposed apartments could help meet these objectives as well as the objective of the Neighborhood Design Policies and Expectations. The intent of these policies and expectations is for residential neighborhood development to provide a well-integrated mix of housing choices with a range of housing types and price levels.

Specifically these policies and expectations call for implementation of five policies:

- Provide pedestrian-friendly neighborhood design with a “sense of place” in which the development is seamlessly integrated with the natural environment;
- Provide flexibility in development standards for lot sizes and setbacks, street design and layout and storm water management to minimize environmental impacts;
- Include a mix of housing choices where no single housing type monopolizes a large area;
- Provide interconnected networks of streets to encourage walking and biking, thus reducing the number and length of automobile trips;
- Require “master plan” development for areas with a variety of housing types to ensure compatibility, long-term value and connectivity.

Although not master planned as part of the surrounding development, the proposed apartments could help meet these objectives. The plan is designed to take advantage of the natural terrain as opposed to utilizing significant fill to raise the site. In doing so large areas of natural environment are preserved and also serve as a buffer to abutting properties. With regard to stormwater and other environmental impacts, the plan meets City stormwater requirements and goals and objectives through the use of best management practices

including detention and treatment via pervious pavement and an infiltration trench and use of large areas of native vegetation. To that extent, Public Works Director Alysén Abel approved the preliminary stormwater plan as compliant with the City adopted regulations, goals and objectives.

The plan promotes “a mix of housing choices where no single housing type monopolizes a large area.” Although proposed to be developed independently of the abutting neighborhoods, the proposed development would provide additional housing choices in the City. The development would also provide a transition between the single-family residential uses east of 9 Highway and the more intense non-residential uses allowed in the zoning south of the site and existing on the west side of 9 Highway.

The proposed site plan provides internal and external sidewalks and trails connecting the development to 9 Highway and to the parkland and 62nd Street to the north. Specifically the plan proposes a sidewalk along the 9 Highway frontage for the length of the property, a trail from the property through City parkland to the north and via a direct sidewalk from the development to 9 Highway. The sidewalks and trails provide access from the development to surrounding uses, provides for future expansion of the sidewalk to the north and south and improves access not only to and from the site, but also to surrounding sites. The proximity to the nearby retail, restaurant, recreation and service uses also encourages walking and has the potential to reduce the number of vehicular trips.

The design also helps meet the policy for ensuring compatibility, long-term value and connectivity. The design reduces the footprint of the project, allowing greater preservation of vegetation and establishing greater than usual setbacks and preservation of open space and vegetation. The proposed apartments would also provide additional residential development helping meet other economic objectives including supporting non-residential uses in the immediate area including the community center, restaurants, retail and service uses in the abutting commercial center.

4. The extent to which the development would be compatible with the surrounding area.

Compatibility is substantially addressed with the associated request for rezoning in agenda item 4A (see items 1 through 3 on pages 2 through 7). In summary the multifamily use provides a transition between the residential uses east of 9 Highway in the Pinecrest and Riss Lake subdivisions and the existing commercial uses west of 9 Highway and potential development on vacant commercial property to the south. Beyond the use, the site design terraces the development down the steep slopes helping minimize the portion of the site affected and leaving more of the site undisturbed than with the previously approved Lake Pointe Professional Centre. The plan also provides greater separation from the nearest residents, greater buffers than previously approved and concentrates parking improvements at the front of the site further away from nearby residents all helping minimize impacts. The building design and use of the site grades also minimizes the height of the structure as viewed from off site.

The applicant has also provided drawings comparing the previously approved site plan to the proposed site plan. The plans demonstrate that the proposed apartment building reduces the lot coverage, increases the open space and vegetation preserved, and has less massing than the previously approved office buildings. They also provide sample views from the nearest abutting residential property to the northeast and from 9 Highway (looking northeast toward the proposed building).

5. **The extent to which the proposal conforms to customary engineering standards used in the City.** Staff concludes that the plan submitted conforms to or can conform to customary engineering standards used in the City and recommends approval subject to noted conditions to ensure that conformance. Site engineering, grading, drainage, sewer and traffic improvements and impacts have been reviewed by Public Works Director Alysen Abel with support from traffic consultant George Butler Associates (GBA) and engineering consultant North Hills Engineering (NHE). Following review, Public Works Director Abel has approved the preliminary stormwater study as compliant with all applicable City regulation and the adopted American Public Works Association (APWA) standards and concluded that the plans meet or exceed minimum requirements and customary engineering standards except as noted. See Public Works Director Alysen Abel's March 27, 2015 memo.

GBA provided the City with an independent review of the proposed traffic impact study (TIS) and driveway geometry. The analyses includes level of service computations, traffic signal warrant worksheets, and MoDOT turn lane warrant worksheets. GBA found that all of the analyses were complete and accurate and concluded that with the exception of needed revisions, the TIS included a thorough analysis of the traffic control and intersection lane configuration needs. Since review the applicant has provided a revised study addressing the prior comments. The study concluded that the volume of traffic does not warrant the installation of a traffic signal at the intersection of Clark Avenue and 9 Hwy, at the entrance to the proposed Lake Pointe Lodge. The study recommended pavement marking modifications for the dual turn lane on 9 Hwy to allow a southbound left turn lane into the project site. The study included recommendations for the geometry of the exiting lanes of the driveway, which include (1) a thru/right lane; and (2) left turn lane, with a minimum of 40 feet of storage in the left turn lane. The study included recommendations for the reconfiguration of the existing Clark Avenue, by converting the exiting eastbound right turn lane to a thru/right lane, allowing access across 9 Hwy to the apartments. GBA and city staff concluded the TIS and site traffic geometry conform to applicable and customary engineering standards, subject to meeting conditions recommended in Public Works Director Alysen Abel's March 27, 2015 memo. See also the February 24, 2015 review letter from Paul M. Bertrand, P.E., PTOE, George Butler Associates, Inc. to Public Works Director Alysen Abel regarding independent review of the proposed traffic study.

Public Works Director Alysen Abel and City engineering consultant Jay Norco of North Hills Engineering Inc. have concluded that the City's gravity sewer system (the piping) has adequate capacity. However, the City's existing station that would accept flow from the development lacks capacity for multi-family development permitted under the proposed R-5 zoning and as proposed in the preliminary development plans. The existing pump station is currently near capacity without additional development. The City currently has improvements scheduled to the pump station as part of the 2015 capital improvement program, including the installation of new pumps. However, to increase the capacity, the developer would be responsible for any cost for larger pumps necessary to serve the development that would be permitted as proposed under the R-5 zoning. See March 27, 2015 memo from Public Works Director Alysen Abel.

Preliminary site engineering standards were also determined to be in conformance with customary engineering standards used in the City. Adequate information has been submitted to demonstrate that the City's minimum standards can be met with noted exceptions where modifications are required, including minor reconfiguration to the site access to accommodate larger vehicles. It should be noted that as of the time of this report the applicant is working to modify the site plan to address this and other identified issues. If

the preliminary development plan is approved, final engineering drawings would be required to be approved prior to final development plan approval to demonstrate that all engineering requirements are met.

6. **The extent to which the location of streets, paths, walkways and driveways are located so as to enhance connectivity, circulation and safety and minimize any adverse traffic impact on the surrounding area.** The plans propose the site entrance in alignment with an extension of Clark Avenue creating an east side of the intersection. It has been concluded that this alignment provides the most logical site access, minimizes conflicts with other intersections and provides adequate sight lines. The TIS also concluded and consultants to the City concurred that:
- pavement marking modifications for the dual turn lane on 9 Hwy are needed to allow a southbound left turn lane into the project site;
 - site entrance should include construction of a thru/right lane and left turn lane, with a minimum of 40 feet of storage in the left turn lane;
 - the exiting eastbound right turn lane of Clark Avenue be converted to a thru/right lane, allowing access across 9 Hwy to the apartments.

The TIS concluded that the volume of traffic does not warrant the installation of a traffic signal at the intersection of Clark Avenue and 9 Hwy, at the entrance to the proposed Lake Pointe Lodge or other immediate location. The City's traffic consultant reviewed the TIS and found that all of the analyses were complete and accurate and concluded that with the exception of needed revisions, the TIS included a thorough analysis of the traffic control and intersection lane configuration needs. City staff concurs and finds that with proposed and recommended approval, the plans minimize traffic impacts on the surrounding area.

Staff also concluded that traffic impacts from the proposed development are less than those from the previously approved Lake Pointe Professional Centre and other anticipated development that could be approved under the existing commercial zoning. The City's traffic consultant, GBA, provided trip generation data for a variety of commercial and residential uses. The data provided the average daily trips for a 50 unit apartment at 427 trips per day or 8.5 trips per unit per day. In comparison the data shows single-family residential trips at a slightly higher but comparable 9.94 trips per unit per day. In contrast, most of the non-residential uses permitted in the existing B-4 zoning district are projected to generate 1.75 to up to 10 times as much traffic. A single drive through bank or the previously approved offices are projected to generate 1.75 to nearly 2 times as much traffic. A single gas station is project to generate over 3.5 times the traffic. A retail center similar in size to the office buildings is projected to generate over 4.5 times the traffic. A single restaurant is projected to generate nearly 3 to 6 times the traffic. A shopping center comparable in size to the office buildings could generate nearly 11 times the traffic of the residential uses permitted in the proposed R-5 zoning. Uses that generate less traffic were other lower density residential uses like senior housing, institutional uses like lodges and churches and lower traffic service uses like self-storage, automotive parts stores and service centers.

With regard to paths and walkways, the proposed site plan provides internal and external sidewalks and trails connecting the development to 9 Highway and to the parkland and 62nd Street to the north. The plans proposed a sidewalk along the 9 Highway frontage for the length of the property, a trail from the property through City parkland to 62nd Street to the north and via a direct sidewalk from the development to 9 Highway. The sidewalks and trails provide access from the development to surrounding uses, provide for future

expansion of the sidewalk to the north and south and improve access not only to and from the site, but also to surrounding sites.

Staff Conclusion and Recommendation

Staff concludes that: the plans meet minimum zoning and subdivision regulations and conform to customary engineering standards used within the City with noted exceptions which can be met as conditions of approval; the site plans cannot be approved without rezoning to the proposed R-5 zoning; the proposed plans can meet several goals and objectives from the City’s adopted Master Plan; the proposed development can be compatible with the surrounding area, and is as compatible as the previously approved development for the same site; and subject to the conditions herein that the location of streets, paths walkways and driveways are located so as to enhance connectivity, circulation and safety and minimize any adverse traffic impact on the surrounding area.

Staff recommends approval of the site plan subject to the conditions noted herein and any additional conditions the Planning and Zoning Commission may deem necessary, if any, to meet the purpose of the R-5 zoning district and to minimize impacts, if any, on nearby development.

It should be noted that the recommendation contained in this report is made without the benefit of being able to consider public comments to be shared during the public hearing. Staff reserves the right to modify or confirm the conclusions and recommendations herein based on consideration of any additional information that may be presented.

Necessary Action

Per Parkville Municipal Code Chapter 426, the proposed site plan is to be approved by the Board of Aldermen following recommendation by the Planning and Zoning Commission following consideration as part of a public hearing.

Following consideration of the proposed preliminary site plan application any additional information presented during the meeting, the Planning Commission should approve or deny the application (with or without conditions) unless otherwise postponed.

	3-27-15
Sean Ackerson, AICP	Date
Assistant City Administrator /	
Community Development Director	



Application #: PZ15-03
 Date Submitted: Jan 30, 15
 Meeting Date: March 10, 15
 Date Approved: _____

CITY OF PARKVILLE • 8880 Clark Avenue • Parkville, MO 64152 • (816) 741-7676 • FAX (816) 741-0018

Site Plan / Development Plan Application

1. Applicant / Contact Information

Applicant(s)

Name: KEVIN GREEN
 Company: KGH BUILDING GROUP LLC
 Address: 6610 ROYAL STREET
 City, State: HERSHEAT VALLEY MO 64068
 Phone: 816 407-7500 Fax: 816 407-7515
 E-mail: KEVIN@KEVINGREENHOMES.COM

Owner(s), if different from applicant

Name: SKCF LLC
 Company: _____
 Address: 6610 ROYAL STREET
 City, State: HERSHEAT VALLEY MO 64068
 Phone: 816 407-7500 Fax: 816 407-7500
 E-mail: KWGREEN@YELP.COM

Person(s) preparing plans

Name: BILL PRELOBAK
 Company: NSPT ARCHITECTS
 Address: 3515 W 75TH STREET
 City, State: PRINCEVILLE KS, SUITE 201
 Phone: 913-831-1415 Fax: 913-831-1563
 E-mail: BILL@PRELOBAKARCH.COM

Contact Person, if different from applicant

Name: _____
 Company: SKCF
 Address: _____
 City, State: _____
 Phone: _____ Fax: _____

We, the undersigned, do hereby authorize the submittal of this application and associated documents and certify that all information contained therein is true and correct. We acknowledge that development in the City of Parkville is subject to the Municipal Code of the City of Parkville. We do hereby agree to abide by and comply with the above-mentioned codes, and further understand that any violations from the provisions of such or from the conditions as stated herein shall constitute cause for fines, punishments and revocation of approvals as applicable.

Applicant's Signature (Required) [Signature] Date: 1/29/2015
 Property Owner's Signature (Required) [Signature] Date: 1/29/2015

2. Property Information

Zoning district: R-5
 Address and general location: EAST OF HIGHWAY 9, SOUTH OF 62ND STREET.
 Legal description (may be attached): - SEE ATTACHMENTS

Description of proposed use(s): 5 UNIT RESIDENTIAL APARTMENT BUILDING CLUBHOUSE WITH OFFICES.

Gross acreage of lot: 6.25 Net acreage of lot: _____
 Open space acreage: _____ Lot coverage: 6.9%
 Is proposal an alteration to an existing building or new construction? alteration new construction

Application #: PZ15-03

3. Factors affecting the project

Are any public improvements required for this project? No

Explain (may be attached): _____

Does the proposed site / development plan meet the following criteria? Attach a separate Yes No sheet explaining how / why.

Does the proposal conform to the provisions of the City's Zoning Code? No - Applying For A ZONING APP. REVISION

Does the proposal conform to the provisions of the City's subdivision regulations? YES

Does the proposal conform to the goals and objectives of the City's Master Plan? YES

Is the proposed development compatible with the surrounding area? YES

Does the proposal conform to customary engineering standards used in the City? YES

Are the streets, paths, walkways and driveways located so as to enhance connectivity, circulation and safety and minimize any adverse traffic impact on the surrounding area? YES

Please note below other comments or factors relating to the proposed development (may be attached):

N/A

4. Checklist of required submittals

- Completed application, including site plan with all required details and supporting data.
- Nonrefundable application fee of \$300.00.
- Five (5) copies 24" x 36" size e, or larger sets, one (1) 11 x 17 size set, and one (1) electronic set (pdf format) of the development plan and elevations for staff and service providers review. Please note additional copies will be required for submittal to the Planning Commission, and Board of Aldermen.
- Five (5) paper copies and one (1), electronic copy (pdf format) of any supporting documentation for staff and service providers review. Please note additional copies will be required for submittal to the Planning Commission, and Board of Aldermen.
- Authorization signature of the applicant and owner of record of the property.
- If subject to covenants and/or deed restrictions, signed approval of the association/entity enforcing such.

For City Use Only

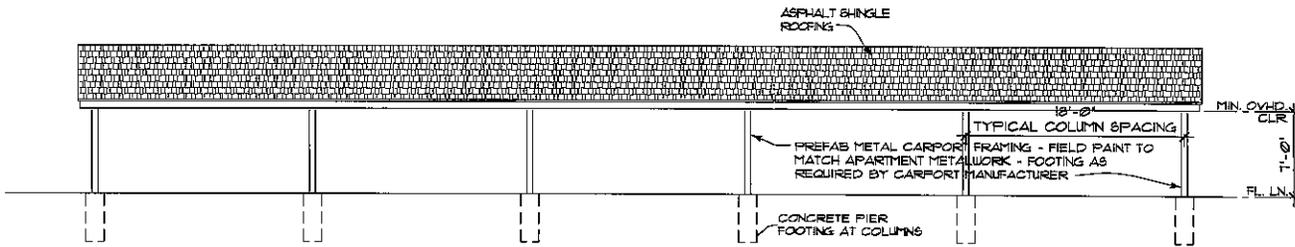
Application accepted as complete by: SEAN ACKERSON / COMM. DEPT. DIR. 2-6-15
Name/Title Date

Application fee payment Check # 27952 M.O. Cash Received from: NSPI ARCHITECTS. \$300.00

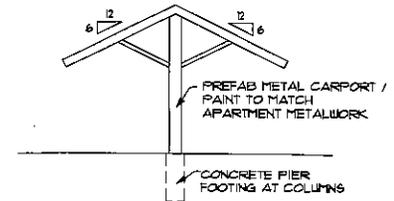
Final reimbursable costs paid (if applicable). Date of Action: _____

Planning Commission Action Approved Approved with Conditions Denied Date of Action: _____
Conditions if any: _____

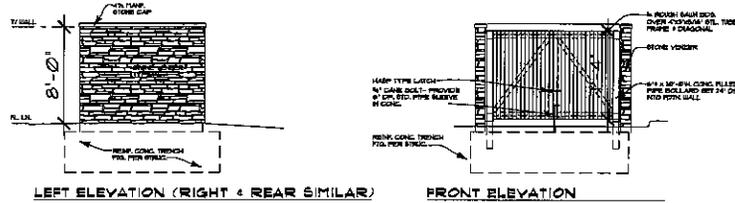
Board of Aldermen Action Approved Approved with Conditions Denied Date of Action: _____
Conditions if any: _____



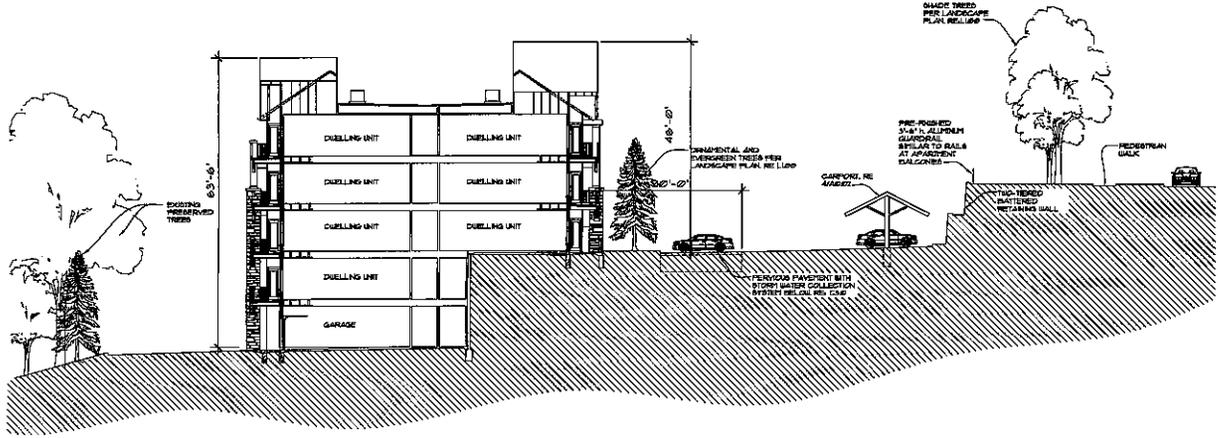
TYPICAL 10-CAR CARPORT FRONT & REAR ELEV. 1/4" = 1'-0"



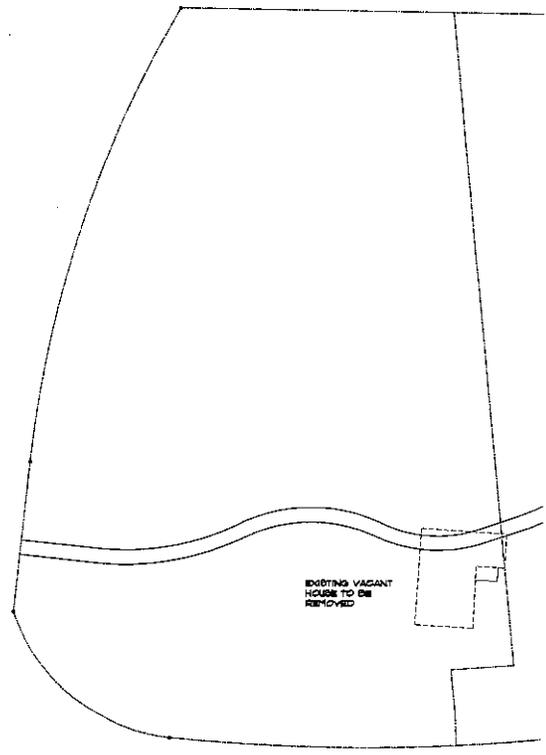
SIDE ELEVATION 1/4" = 1'-0"



TRASH COMPACTOR ENCLOSURE 1/4" = 1'-0"



PRELIMINARY SITE SECTION SCALE 1/8" = 1'-0"



PRELIMINARY SITE PLAN-NORTH LOT SCALE 1" = 10'-0"

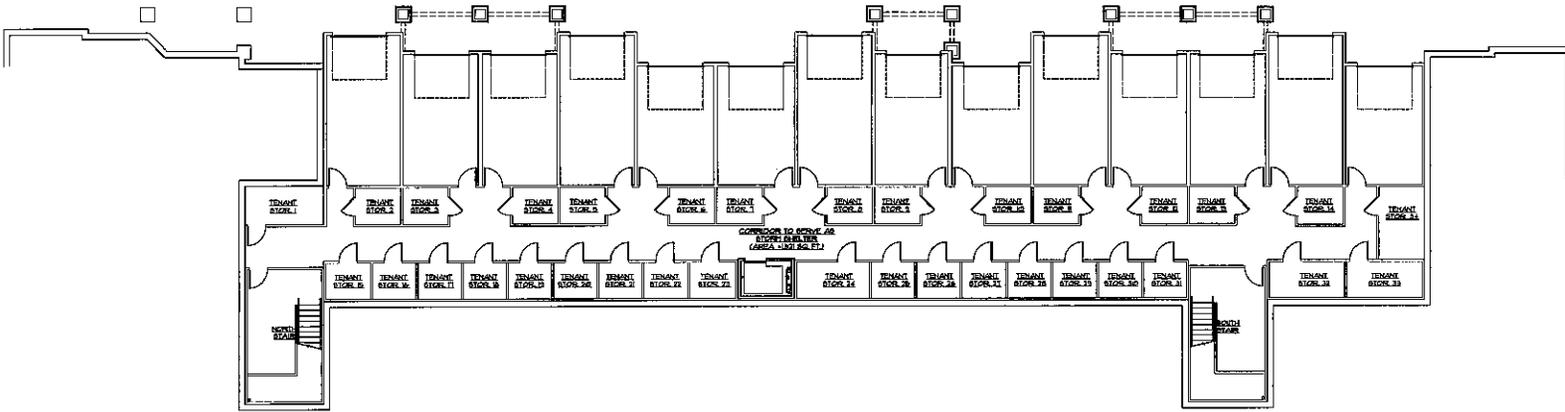
LAKE POINTE LODGE
 6115 and 6105 HIGHWAY 9
 PARKVILLE, MISSOURI

NSPJ
 NEARING STAATS FRELIGAR & JONES AIA CHARTERED ARCHITECTS
 3833 W. 51st St., Ste. 201
 PARKVILLE, MISSOURI 64088
 www.nspjarch.com
 Phone: 913.651.1411 Fax: 913.651.1500

REVISIONS
 DATE: January 30, 2014
 JOB NO: 58371
 SHEET NO:

A0.02

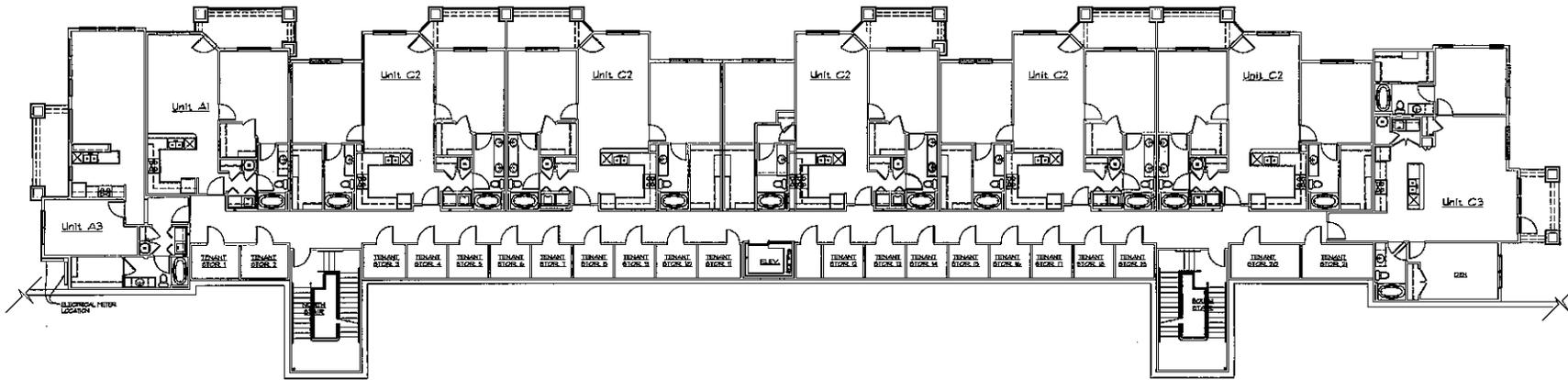
03.25.15 PRELIMINARY DEVELOPMENT PLAN RESUBMITTAL



LOWER LEVEL LAYOUT PLAN
 1/8" = 1'-0"

LAKE POINTE LODGE
 6115 and 6105 HIGHWAY 9
 PARKVILLE, MISSOURI

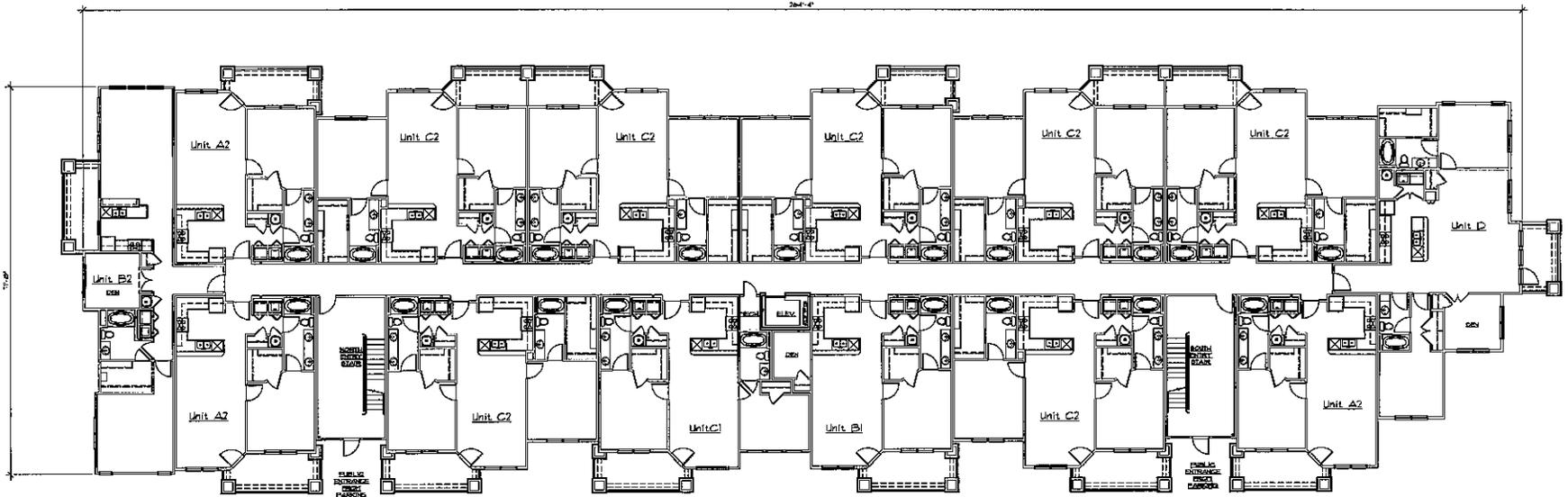
REVISIONS
 Date: January 30, 2014
 Job No.: 508713
 Sheet No.:



FIRST FLOOR LAYOUT PLAN
1/8" = 1'-0"

Revisions

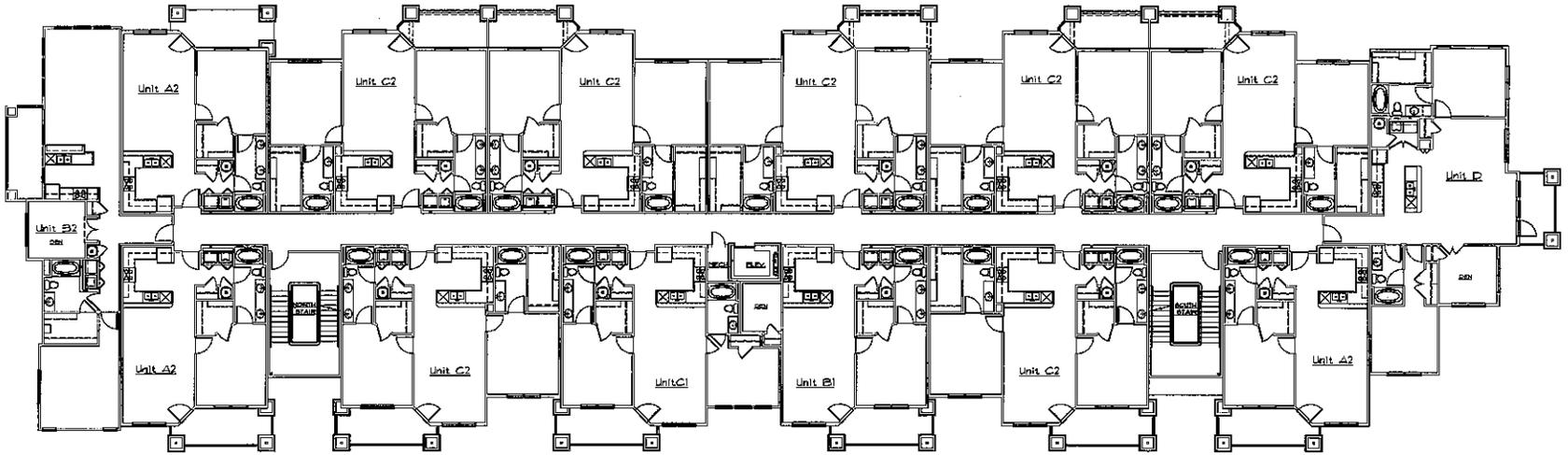
Date:
January 30, 2014
Job No:
183711
Sheet No.



2ND FLOOR LAYOUT PLAN
 1/8" = 1'-0" (ENTRY LEVEL)

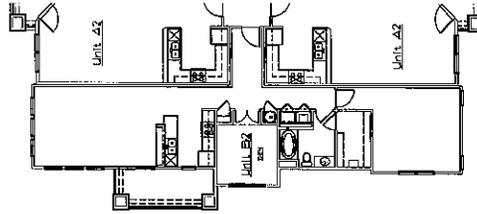
LAKE POINTE LODGE
 6115 and 6105 HIGHWAY 9
 PARKVILLE, MISSOURI

REV: 03/05/06
 DATE: January 30, 2014
 JOB NO: 033711
 SHEET NO:

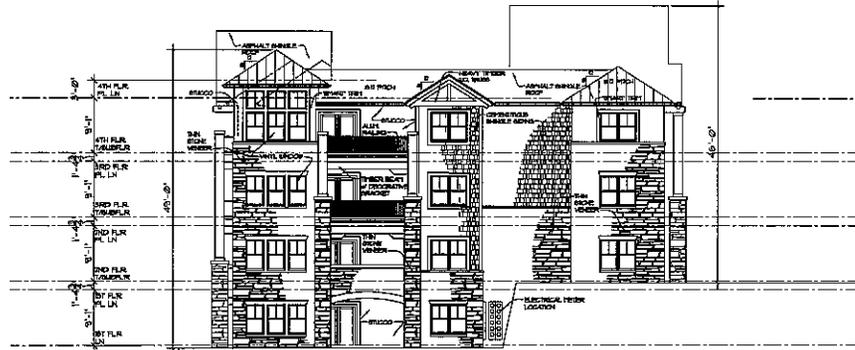


3RD & 4TH FLOOR LAYOUT PLAN
1/8" = 1'-0"

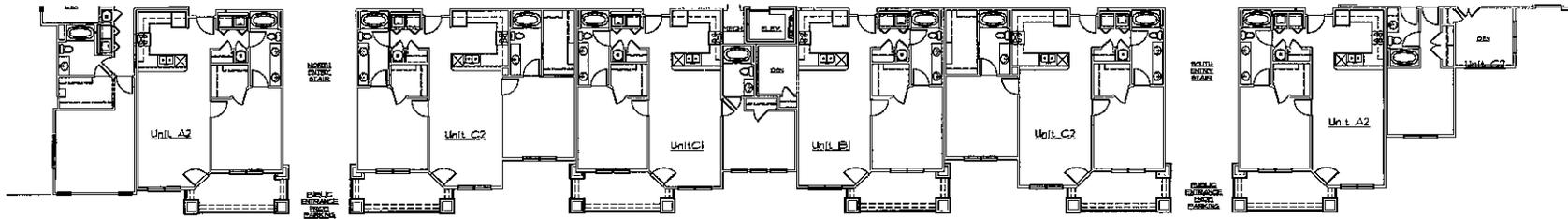
REVISIONS
 Date: January 30, 2014
 Job No: 54771
 Sheet No:



PARTIAL REFERENCE PLAN
1/8" = 1'-0"



LEFT (NORTH) ELEVATION
1/8" = 1'-0"



PARTIAL REFERENCE 2ND FLOOR PLAN
1/8" = 1'-0"



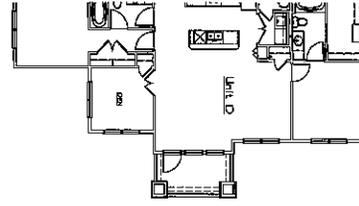
FRONT (WEST) ELEVATION
1/8" = 1'-0"

NSPJ
NEARING STRAITS PRELOGAR & JONES AIA CHARTERED
ARCHITECTS
3535 W. 79th St., Suite 201
Overland Park, MO 66212
Phone: 781.333.1900
www.nspjarch.com
PARKVILLE, MISSOURI 64088

LAKE POINTE LODGE
6115 and 6105 HIGHWAY 9
PARKVILLE, MISSOURI

Rev: jcd

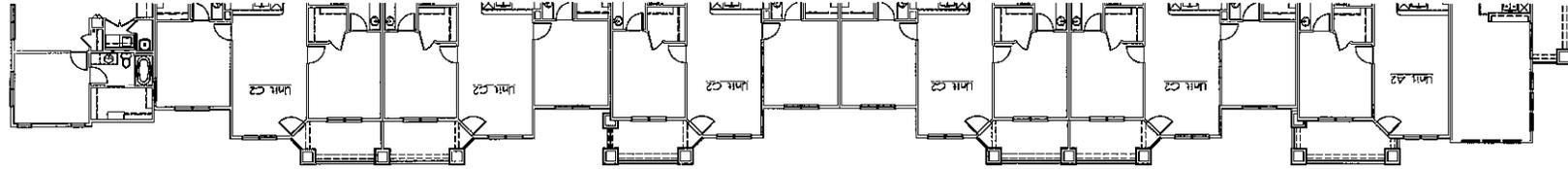
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JOB NO: 26771
SHEET NO:



PARTIAL REFERENCE 2ND FLOOR PLAN
 1/8" = 1'-0"



RIGHT (SOUTH) ELEVATION
 1/8" = 1'-0"



PARTIAL REFERENCE 2ND FLOOR PLAN
 1/8" = 1'-0"

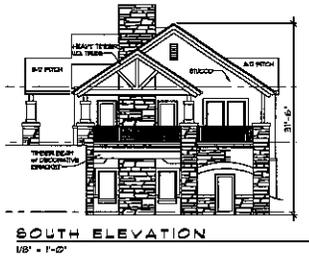


REAR (EAST) ELEVATION
 1/8" = 1'-0"

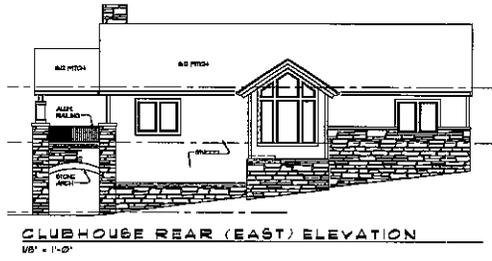
LAKE POINTE LODGE
 6115 and 6105 HIGHWAY 9
 PARKVILLE, MISSOURI

REVISIONS

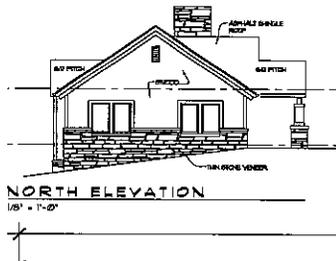
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 Sheet No.



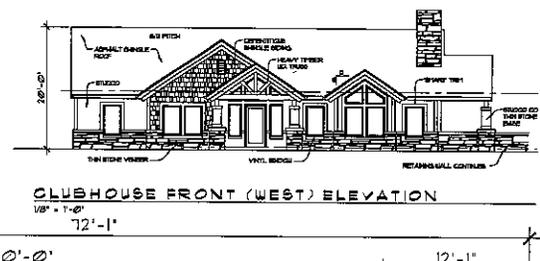
SOUTH ELEVATION
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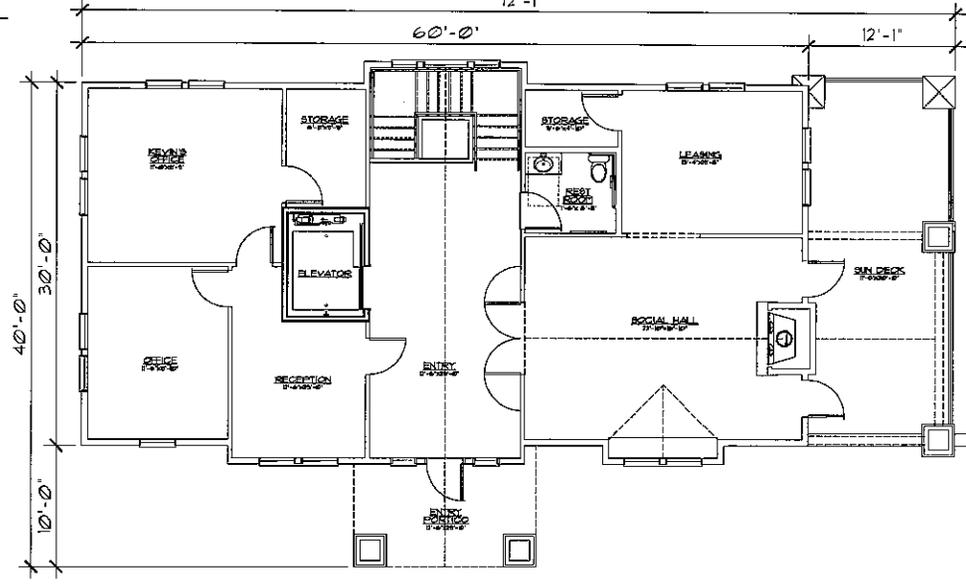
CLUBHOUSE REAR (EAST) ELEVATION
1/8" = 1'-0"



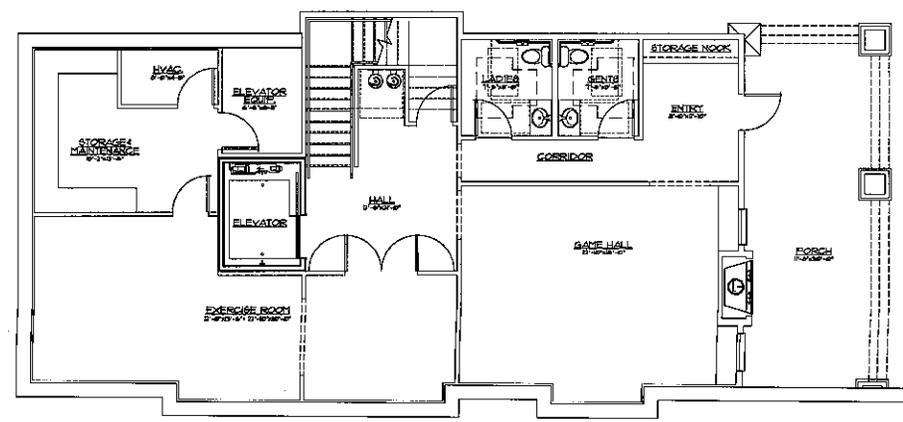
NORTH ELEVATION
1/8" = 1'-0"



CLUBHOUSE FRONT (WEST) ELEVATION
1/8" = 1'-0"



1ST FLOOR CLUBHOUSE PLAN
1/4" = 1'-0"



LOWER LEVEL CLUBHOUSE PLAN
1/4" = 1'-0"

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20552750, SUITE 1000 WWW.NSPJARCHITECTS.COM
PARKVILLE, MISSOURI 64079 PHONE 816.224.1015 FAX 816.224.1080

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6115 and 6105 HIGHWAY 9
PARKVILLE, MISSOURI

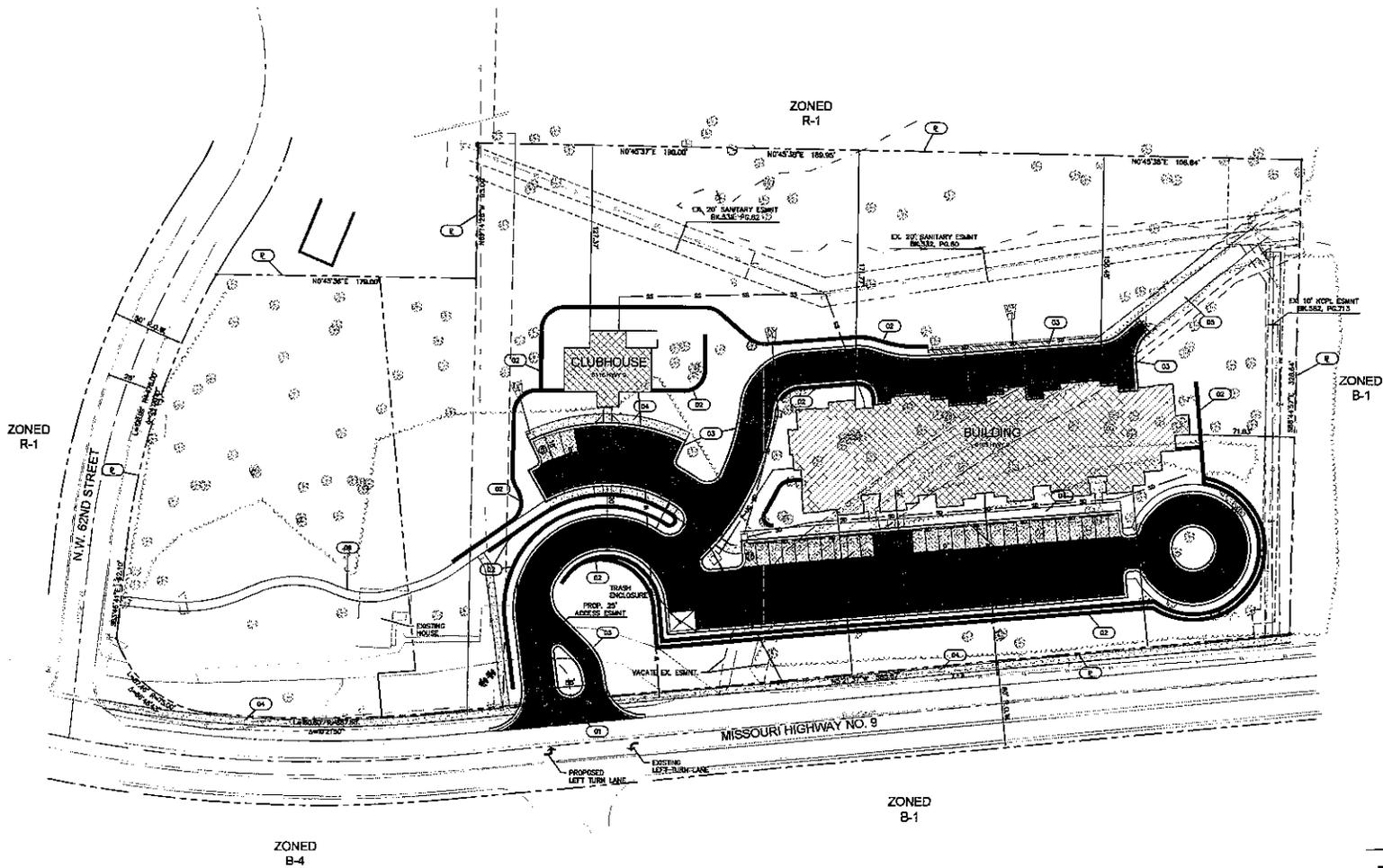
REVISIONS

Date: January 30, 2014
Job No.: SB011
Sheet No.:

NOTES:
 THE INCLUDED PLANT LIST IS A REPRESENTATION OF A RANGE OF POTENTIAL PLANT MATERIAL AND IS NOT INTENDED AS THE FINAL SELECTION OF PLANT VARIETIES TO BE INCLUDED.

PRELIMINARY PLANT LIST

Common Name	Botanical Name	Size	Notes
Shade Trees			
001	American Elm	2" Cal.	36' H
002	Boxelder	2" Cal.	36' H
003	Common Goldeneye	2" Cal.	36' H
004	Green Ash	2" Cal.	36' H
005	Red Oak	2" Cal.	36' H
006	White Oak	2" Cal.	36' H
007	Black Oak	2" Cal.	36' H
008	White Birch	2" Cal.	36' H
009	Yellow Birch	2" Cal.	36' H
010	Black Birch	2" Cal.	36' H
011	Red Birch	2" Cal.	36' H
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103	Black Birch	2" Cal.	36' H
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273	White Birch	2" Cal.	36' H
274	Black Birch	2" Cal.	36' H
275	Red Birch	2" Cal.	36' H
27			



- LEGEND**
- PROPERTY LINE
 - 100YR FLOODPLAIN LIMITS
 - - - - - EXISTING SANITARY SEWER
 - EXISTING WATER
 - EXISTING UNDERGROUND POWER LINE
 - P-400 PROPOSED ELECTRIC SERVICE
 - W PROPOSED WATER SERVICE
 - S-18 PROPOSED SANITARY SEWER
 - S-20 PROPOSED SEWAGE SEWER
 - Ⓟ PARKING STALL COUNT

- KEYNOTES**
- Ⓧ PROPERTY LINE
 - 01 DRIVE APPROACH PER PUBLIC ROAD PLANS
 - 02 PROPOSED RETAINING WALL TYP.
 - 03 PROPOSED CURB AND GUTTER
 - 04 PROPOSED CONCRETE SIDEWALK
 - 05 PUMP STATION ACCESS
 - 06 PROPOSED PEDESTRIAN PATH

DESCRIPTION
 LOTS 1 AND 2 OF LAKE POINTE PROFESSIONAL CENTRE SUBDIVISION, LOCATED IN THE NORTHEAST QUARTER OF SECTION 28, TOWNSHIP 51 NORTH, RANGE 34 WEST IN PARKVILLE, PLATE COUNTY, MISSOURI.



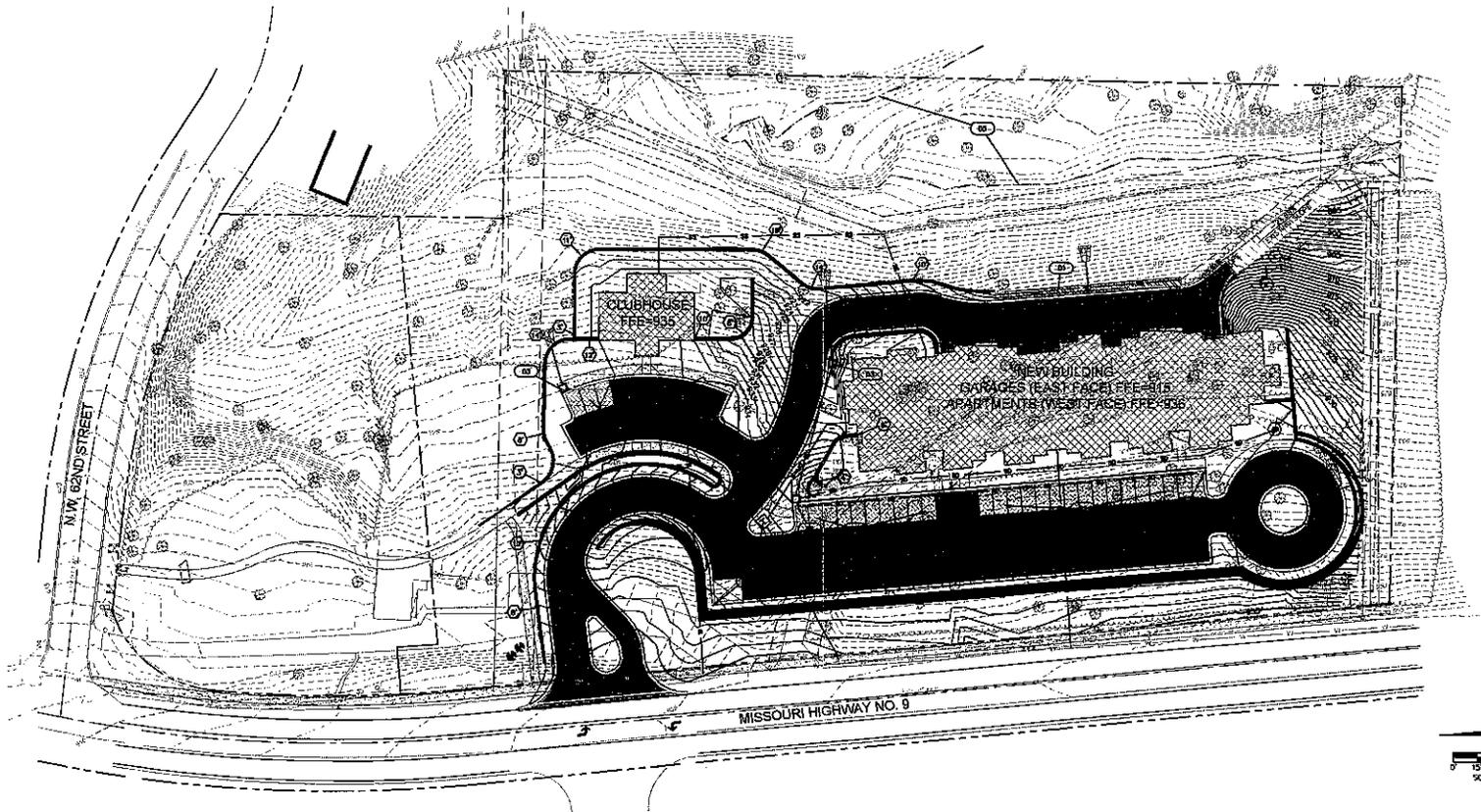
NSPJ
 NEARING STUARTS PRELOGAR & JONIS AIA CHARTERED ARCHITECTS
 3850 W. 54th St. Suite 100
 Parkville, MO 64151
 Phone: (816) 221-1111 Fax: (816) 221-1112

LAKE POINTE LODGE
 6115 and 6105 HIGHWAY 9
 PARKVILLE, MISSOURI

SITE PLAN

03.25.15 PRELIMINARY DEVELOPMENT PLAN RESUBMITTAL

C1.0

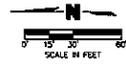


LEGEND

- PROPERTY LINE
- 100YR FLOODPLAIN LIMITS
- EXISTING SANITARY SEWER
- EXISTING WATER
- EXISTING UNDERGROUND POWER LINE
- PROPOSED UNDERGROUND ELECTRIC SERVICE
- PROPOSED WATER SERVICE
- PROPOSED SANITARY SEWER
- PROPOSED STORM SEWER

KEYNOTES

- R. PROPERTY LINE
- 01. PROPOSED LEVEL SPREADER
- 02. PROPOSED PERVIOUS PAVEMENT WITH STORAGE
- 03. PROPOSED CURB INLET
- 04. PROPOSED POWER TRANSFORMER
- 05. 100YR FLOODPLAIN LIMITS
- ⊙ WALL HEIGHTS



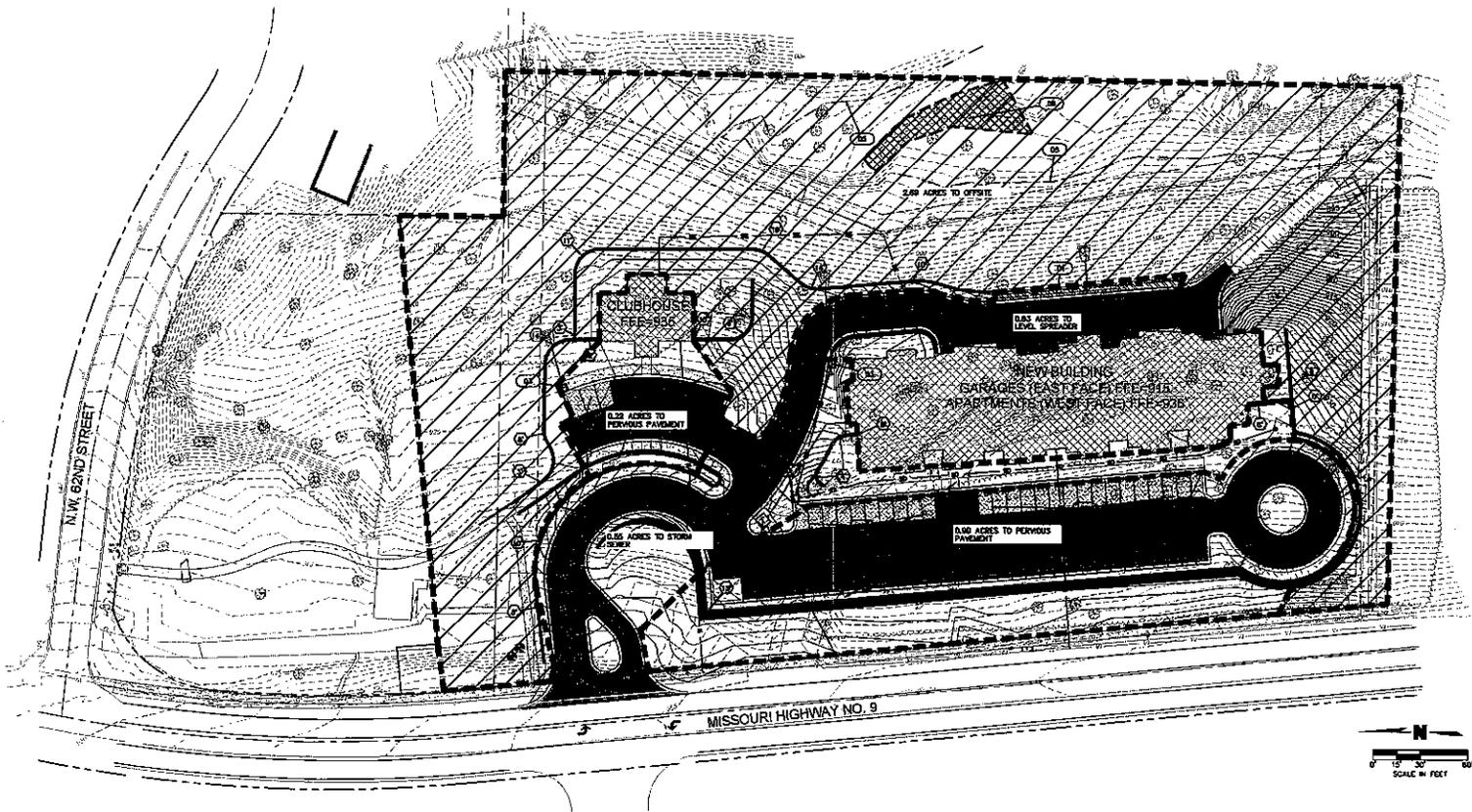
NSPJ
 NEARING STAIRS PERLOGAR & JONES AIA CHARTERED
 ARCHITECTS
 200 W. 10TH ST. SUITE 200
 PARKVILLE, MISSOURI 64151
 WWW.NSPJARCH.COM
 PHONE: 816.221.1100 FAX: 816.221.1101

LAKE POINTE LODGE
 6115 and 6105 HIGHWAY 9
 PARKVILLE, MISSOURI

GRADING & UTILITY PLAN

03.25.15 PRELIMINARY DEVELOPMENT PLAN RESUBMITTAL

C2.0

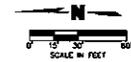


LEGEND

---	PROPERTY LINE
---	100YR FLOODPLAIN LIMITS
---	EXISTING SANITARY SEWER
---	EXISTING WATER
---	EXISTING UNDERGROUND POWER LINE
---	PROPOSED ELECTRIC SERVICE
---	PROPOSED WATER SERVICE
---	PROPOSED SANITARY SEWER
---	PROPOSED STORM SEWER
---	PROPOSED DRAINAGE BOUNDARY

KEYNOTES

Ⓢ	PROPERTY LINE
01	PROPOSED LEVEL SPREADER
02	PROPOSED PERVIOUS PAVEMENT WITH STORAGE
03	PROPOSED CURB INLET
04	PROPOSED POWER TRANSFORMER
05	100YR FLOODPLAIN LIMITS
08	SEEDMENT TRAP
Ⓜ	WALL HEIGHTS



NSPI
 NEARING STAATS PELOGAR & JONES AIA CHARTERED
 ARCHITECTS
 315 WEST 15th STREET, SUITE 200
 PARKVILLE, MISSOURI 64151-1500
 PHONE: 816.221.1100 FAX: 816.221.1100

LAKE POINTE LODGE
 6115 and 6105 HIGHWAY 9
 PARKVILLE, MISSOURI

STORMWATER MANAGEMENT PLAN

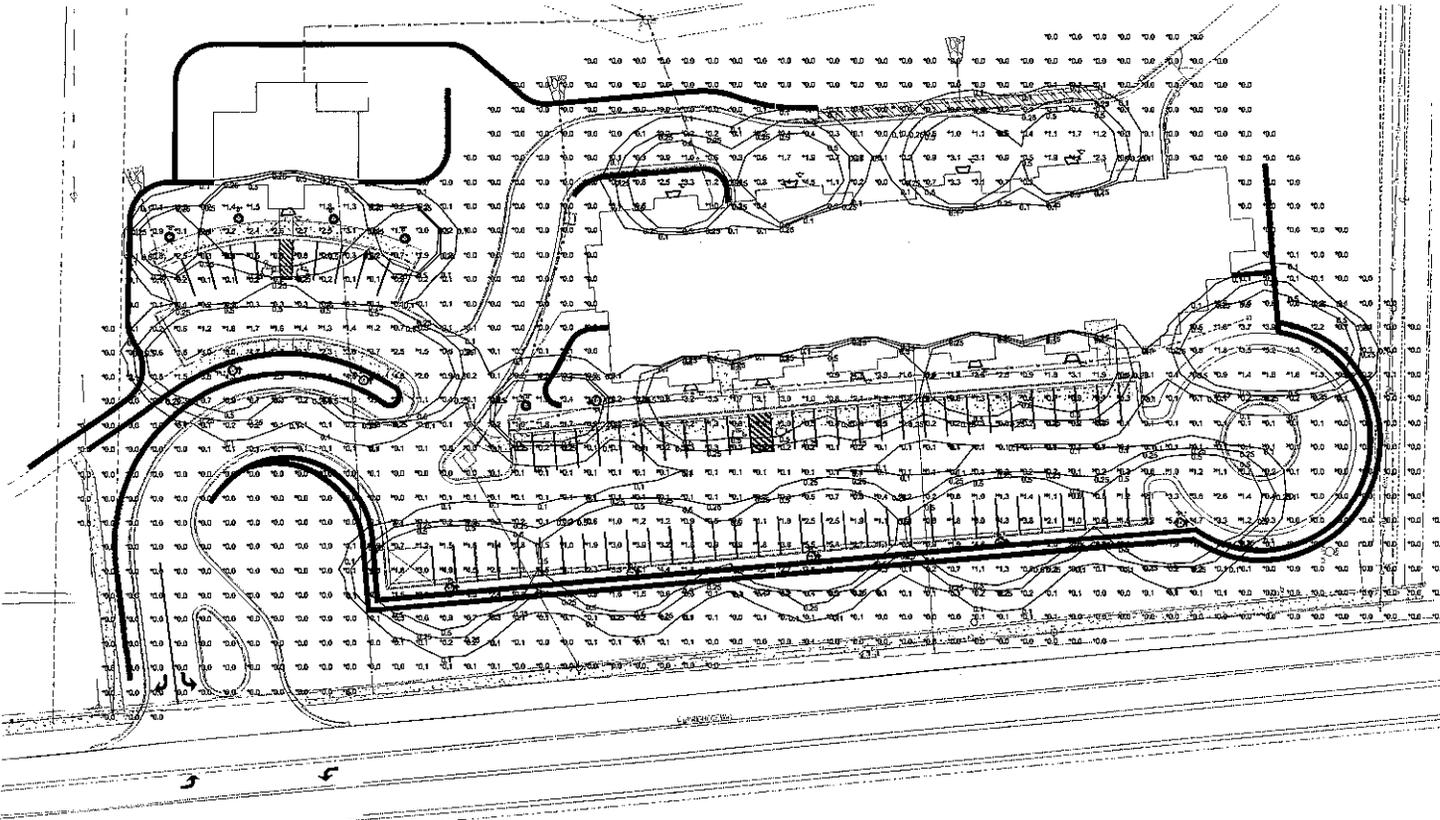
03.25.15 PRELIMINARY DEVELOPMENT PLAN RESUBMITTAL

C3.0

SYMBOL	TYPE	SIZE	QUANTITY	DESCRIPTION	MANUFACTURER	MODEL	W.H.	W.L.	W.H.
☐	R	L	1	LED POLE TOP AMBIENT REFLECTOR LED WITH OPTIC	APPLICATIONS	LED21	1420	363.0	0.9
○	B	S	8122	RECESSED W/TH HOLLOW AND CLEAR PLASTER LUM	APPLICATIONS	ONE LIGHT USE IN WALL IN LAMP	1420	363.0	0.9
□	C	21	50	LED LINE SOURCE	PHILIPS BARCO	QUANTIFLUX LUMINOUS	1420	363.0	0.9

SYMBOL	TYPE	SIZE	QUANTITY	DESCRIPTION	MANUFACTURER	MODEL	W.H.	W.L.	W.H.
○	B	S	8122	RECESSED W/TH HOLLOW AND CLEAR PLASTER LUM	APPLICATIONS	ONE LIGHT USE IN WALL IN LAMP	1420	363.0	0.9
□	C	21	50	LED LINE SOURCE	PHILIPS BARCO	QUANTIFLUX LUMINOUS	1420	363.0	0.9

NOTE: 0.1 FEET WERE USED AS AN ESTIMATION FOR POSSIBLE BALANCE POINTS. PLEASE REFER TO BIDDING DOCUMENTS FOR ALL OTHER RELATED LUMINAIRES.



1 SITE PHOTOMETRICS PLAN
SCALE: 1" = 30'

Specifications LA792

LA792
Luminaire Specifications

SYMBOL	TYPE	SIZE	QUANTITY	DESCRIPTION	MANUFACTURER	MODEL	W.H.	W.L.	W.H.
☐	R	L	1	LED POLE TOP AMBIENT REFLECTOR LED WITH OPTIC	APPLICATIONS	LED21	1420	363.0	0.9
○	B	S	8122	RECESSED W/TH HOLLOW AND CLEAR PLASTER LUM	APPLICATIONS	ONE LIGHT USE IN WALL IN LAMP	1420	363.0	0.9
□	C	21	50	LED LINE SOURCE	PHILIPS BARCO	QUANTIFLUX LUMINOUS	1420	363.0	0.9

Specifications BL0210

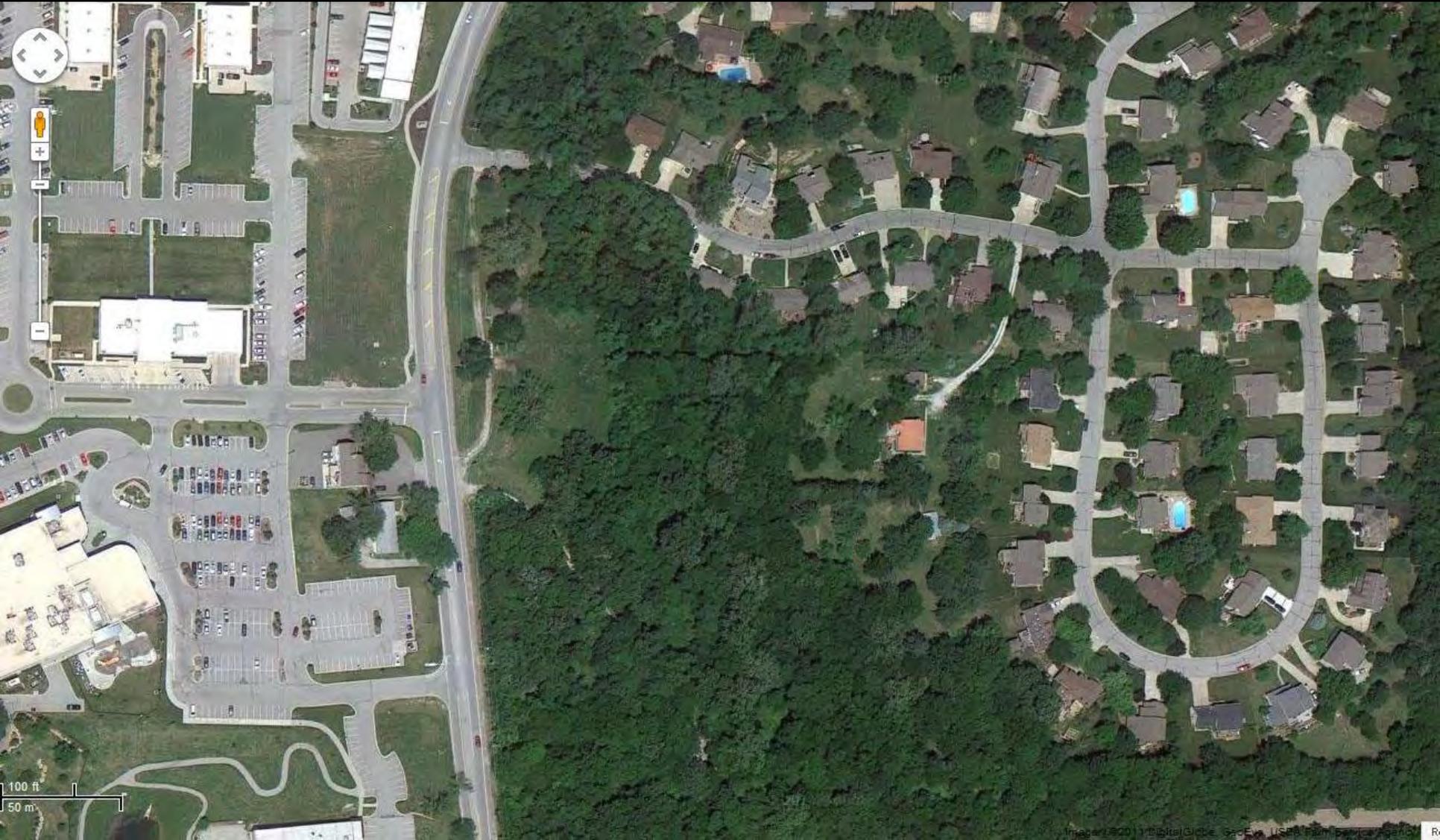
BL0210
Luminaire Specifications

SYMBOL	TYPE	SIZE	QUANTITY	DESCRIPTION	MANUFACTURER	MODEL	W.H.	W.L.	W.H.
○	B	S	8122	RECESSED W/TH HOLLOW AND CLEAR PLASTER LUM	APPLICATIONS	ONE LIGHT USE IN WALL IN LAMP	1420	363.0	0.9
□	C	21	50	LED LINE SOURCE	PHILIPS BARCO	QUANTIFLUX LUMINOUS	1420	363.0	0.9

NSPJ
NEARING STAAFS FREGOGAR & JONES AIA CHARTERED ARCHITECTS

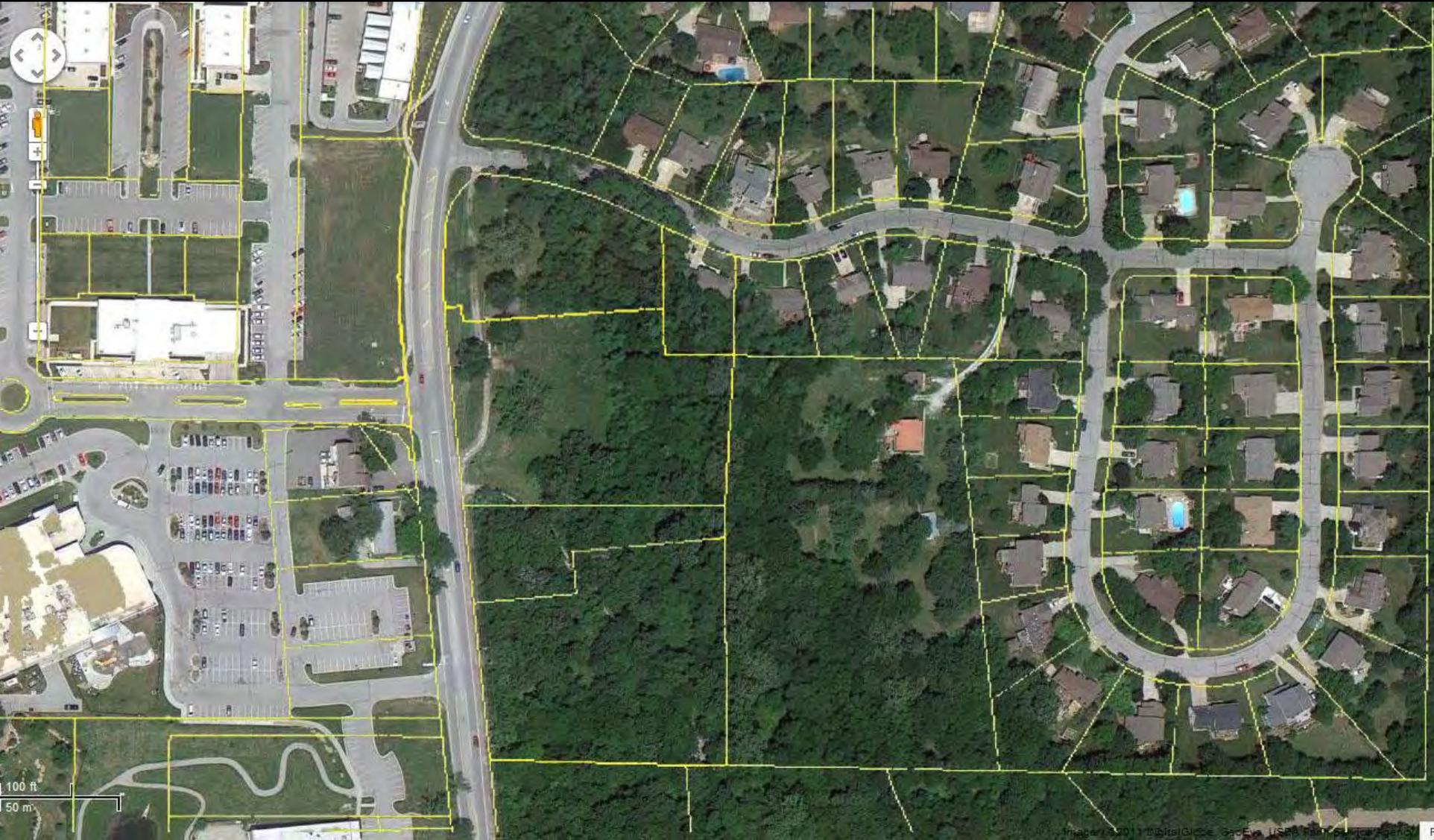
LAKE POINTE LODGE
NEAR CLARK AVE. & HIGHWAY 9
PARKVILLE, MISSOURI

A PROPOSED DEVELOPMENT
in
PARKVILLE, MO



100 ft
50 m

A PROPOSED DEVELOPMENT
in
PARKVILLE, MO



A PROPOSED DEVELOPMENT
in
PARKVILLE, MO



6.25 Acres
272,250 SFT

100 ft
50 m



PROPOSALS



PREVIOUSLY APPROVED



CURRENTLY PROPOSED

BUILDING FOOTPRINT



28,232 SQFT



18,907 SQFT

DIFFERENCE IN PROPOSALS



9,325 SQFT

PAVED SURFACE AREA



78,735 SQFT



46,895 SQFT

DIFFERENCE IN PROPOSALS



31,840 SQFT

PRESERVED SITE AREA



101,978 SQFT



158,126 SQFT

DIFFERENCE IN PROPOSALS



56,148 SQFT

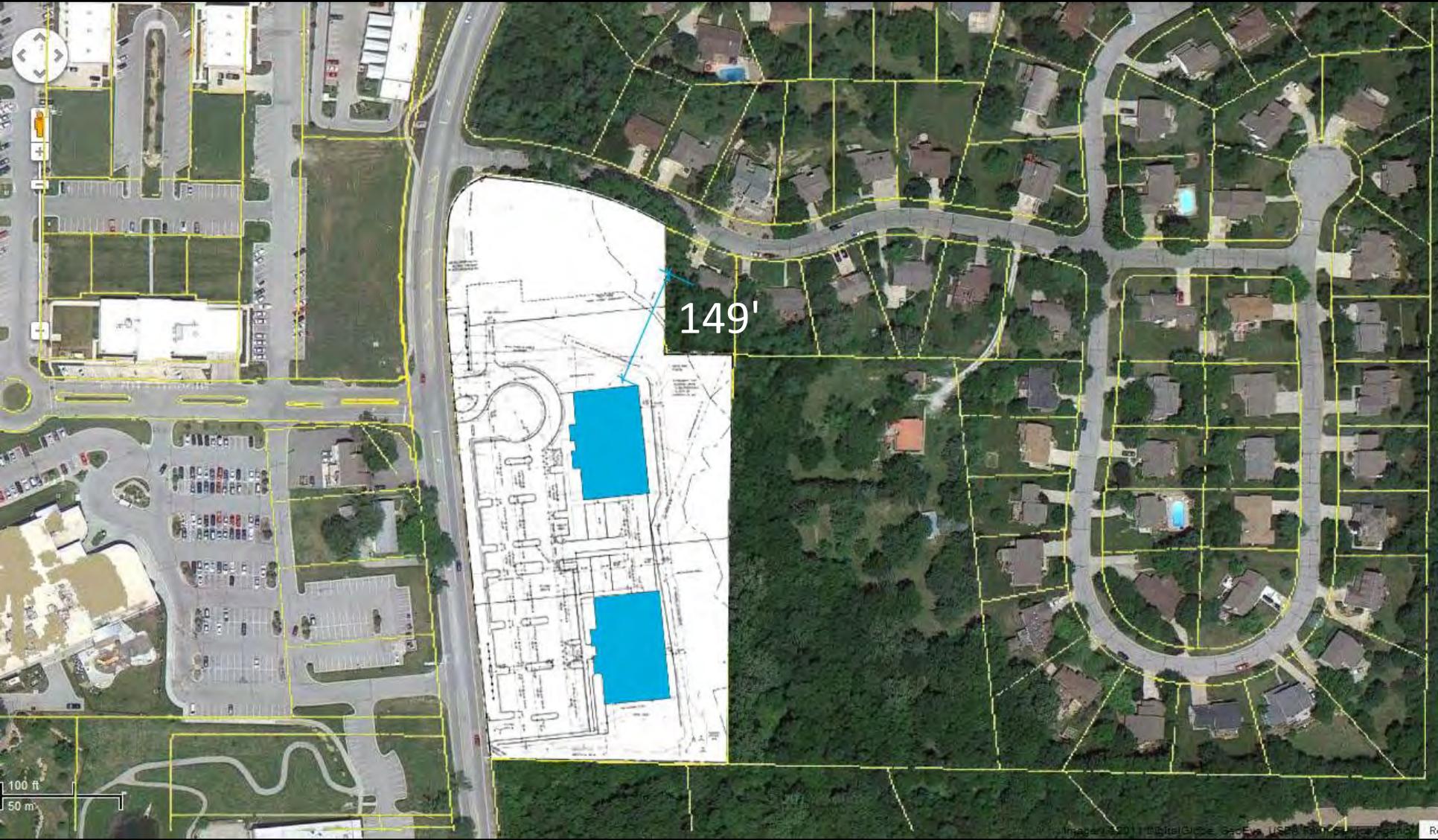


PREVIOUSLY APPROVED



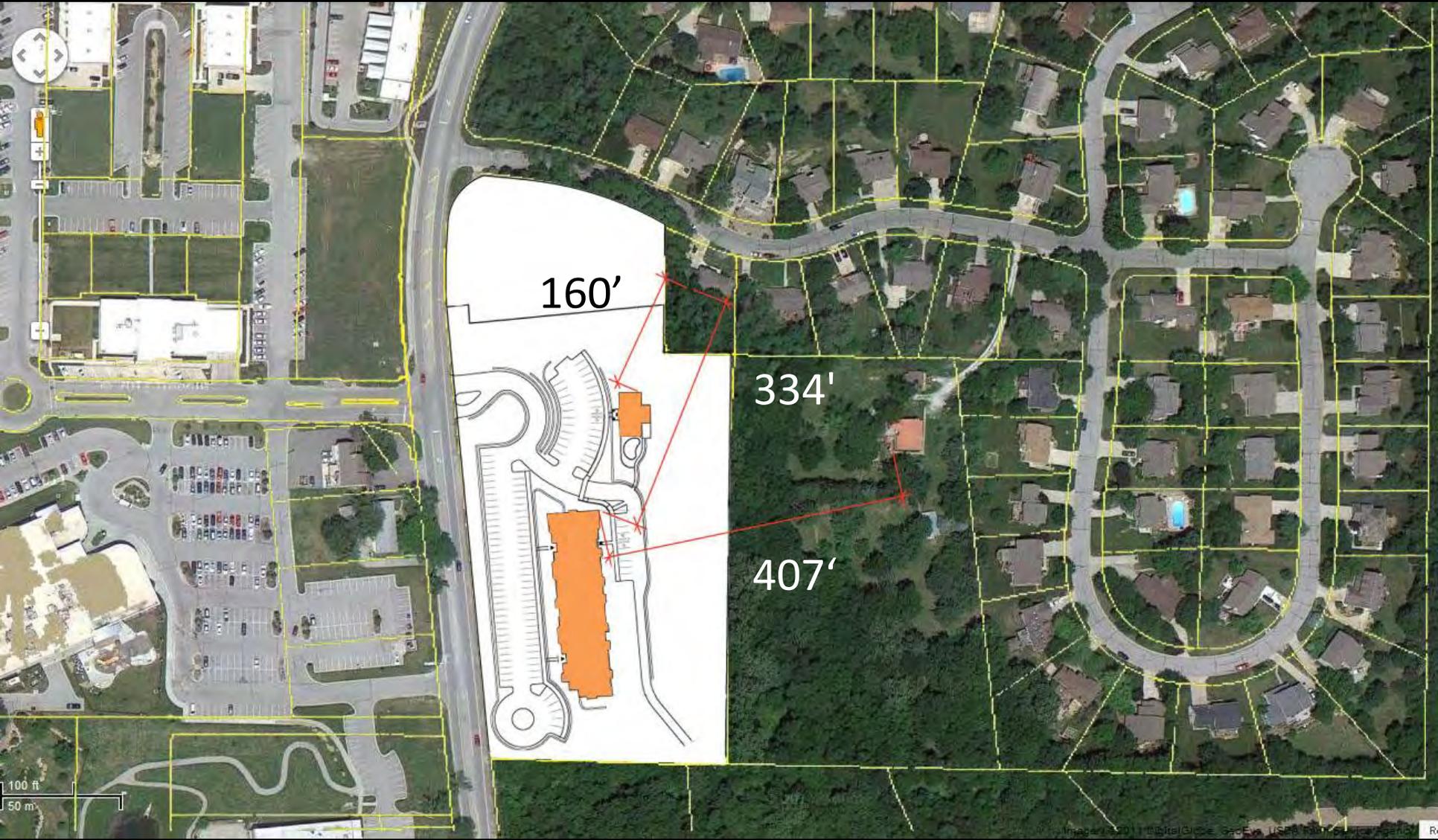
CURRENTLY PROPOSED

DISTANCE TO NEAREST NEIGHBOR

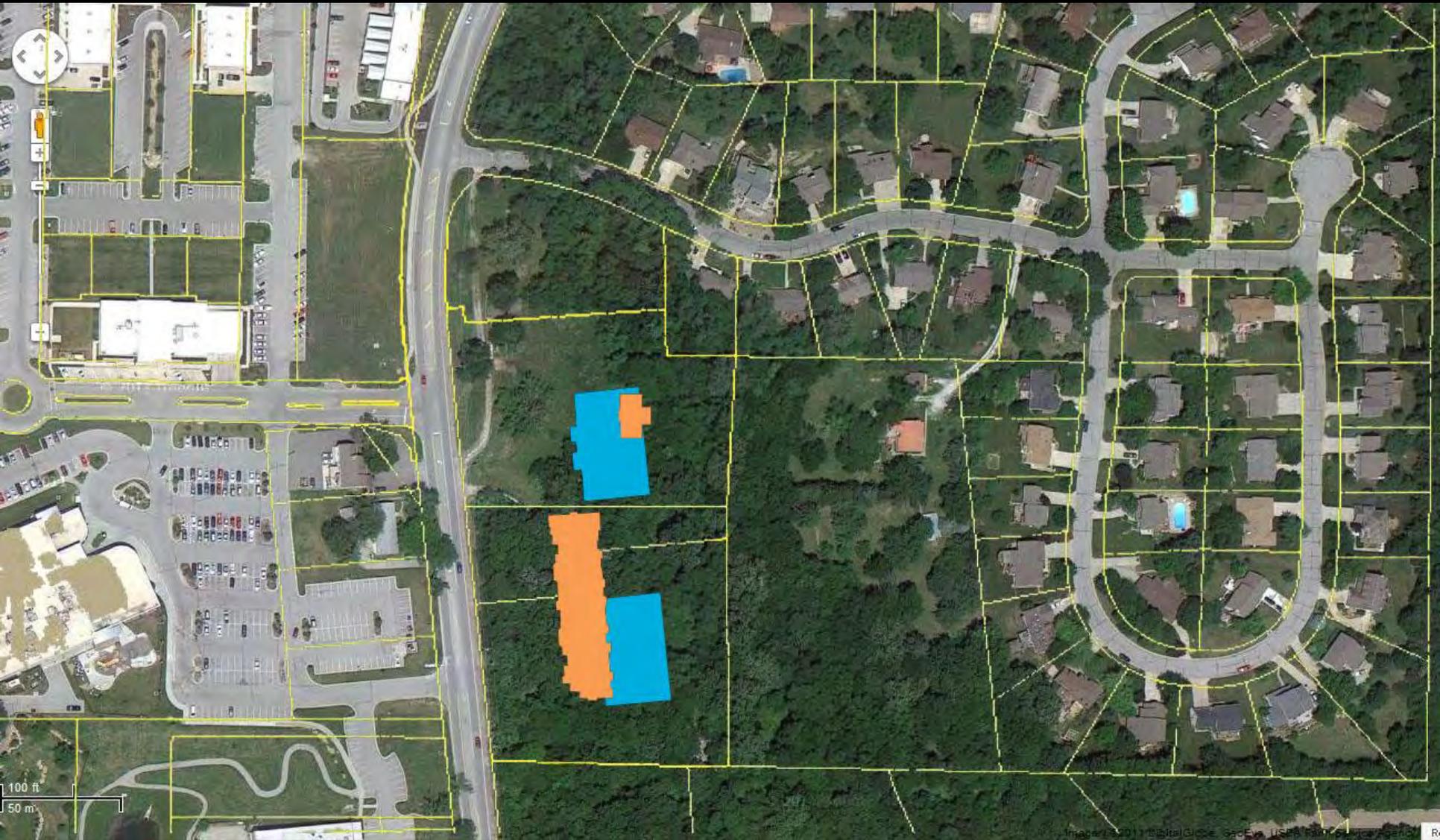


PREVIOUSLY APPROVED

DISTANCE TO NEAREST NEIGHBOR



CURRENTLY PROPOSED



100 ft
50 m

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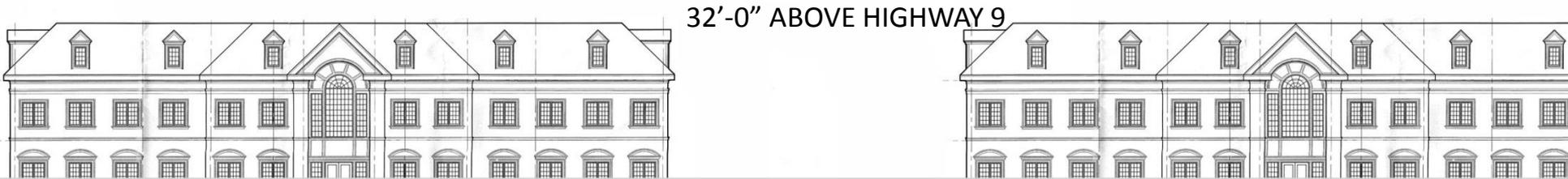
PREVIOUS PROPOSED FRONT (WEST) ELEVATION



44' ABOVE GRADE



PREVIOUS PROPOSED FRONT (WEST) ELEVATION

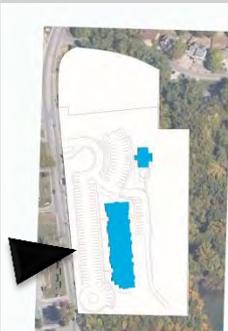


CURRENTLY PROPOSED FRONT (WEST) ELEVATION

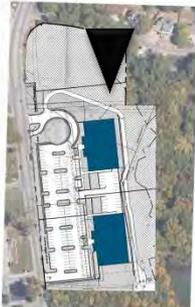
46' ABOVE GRADE



CURRENTLY PROPOSED FRONT (WEST) ELEVATION



PREVIOUS PROPOSED NORTH SIDE ELEVATION

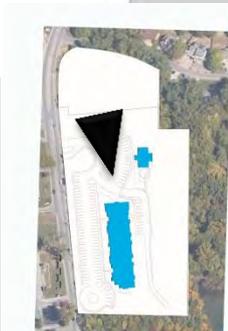


CURRENTLY PROPOSED NORTH SIDE ELEVATION

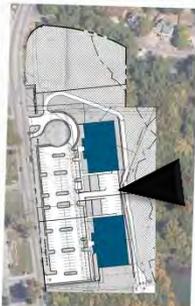
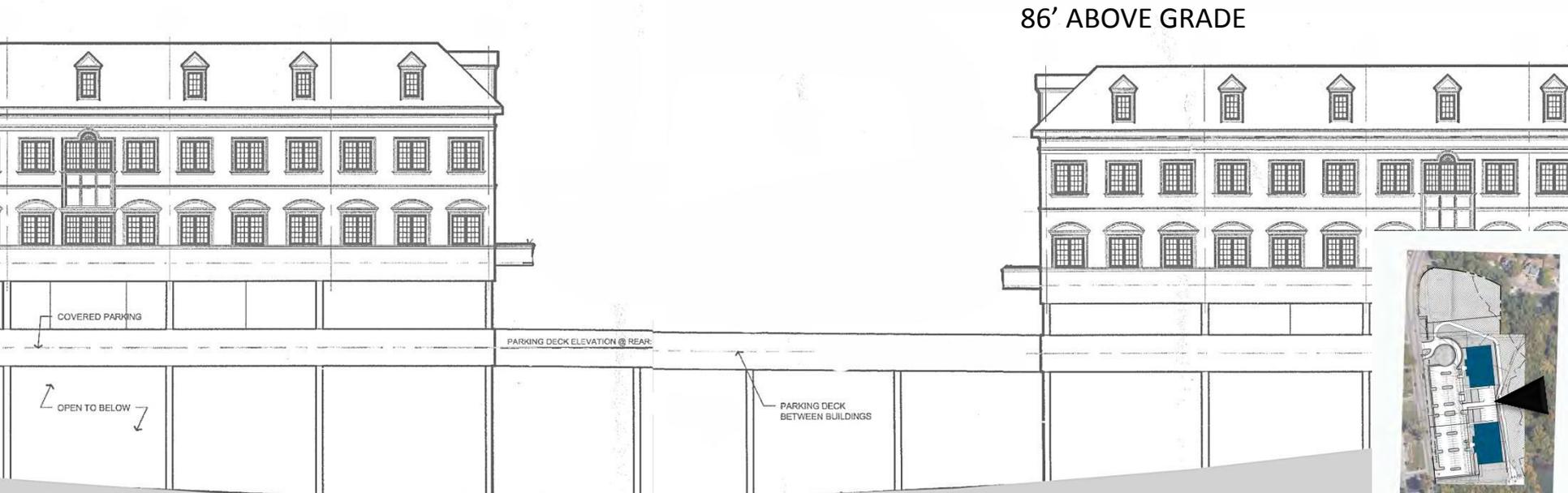
49' ABOVE GRADE



46' ABOVE GRADE



PREVIOUS PROPOSED REAR (EAST) ELEVATION



CURRENTLY PROPOSED REAR (EAST) ELEVATION

63'-6" ABOVE GRADE



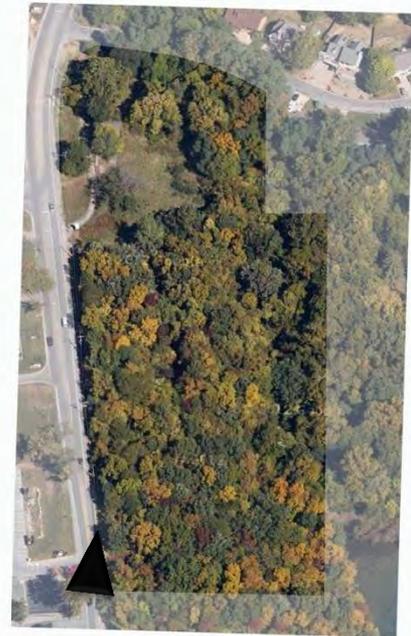
CURRENTLY PROPOSED SOUTH SIDE ELEVATION





EXISTING SITE CONDITION

January 18 2012





EXISTING SITE CONDITION

January 18 2012





EXISTING SITE CONDITION

January 18 2012





EXISTING SITE CONDITION

January 18 2012



VIEW FROM NEAREST NEIGHBOR



VIEW FROM HIGHWAY 9



EXAMPLE OF PROJECT WITH SIMILAR EXTERIOR MATERIALS



EXAMPLE OF PROJECT WITH SIMILAR EXTERIOR MATERIALS



EXAMPLE OF PROJECT WITH SIMILAR EXTERIOR MATERIALS

